



MMRP – Constructability Advisor

Memorandum - Confidential & Subject to Legal Professional Privilege

To:	Tim Power (HSF)	Date:	26 August 2016
CC:	Scott Thompson	From:	Jonathan Wilcox
Subject:	Western Portal – Pedestrian Arrangements and South Kensington Station Access- Southern Entrance		
Doc No:	MMR_ADV_WP_0002		

1 Introduction

1.1 Purpose

The purpose of this memorandum is to provide information for the Melbourne Metro Rail Environmental Effect Statement (EES) panel hearing. It responds to a request from MMRA’s legal advisor Herbert Smith Freehills (HSF) on 25 August 2016 to provide:

- A plan of pedestrian path closures at the Western Portal under both portal options;
- Plans and a cross section to show how access will be provided to South Kensington station during construction under both portal options,

1.2 Acronyms & Abbreviations

Table 1-1 contains the acronyms and abbreviations used in this memorandum.

Table 1-1: Memorandum Acronyms and Abbreviations

TERMINOLOGY & ACRONYMS	
AJM JV	Aurecon Jacobs Mott Macdonald Joint Venture
EES	Environmental Effects Statement
HSF	Herbert Smith Freehills – MMRA Legal Advisor
MMRA	Melbourne Metro Rail Authority



2 Background

Two options have been considered in the EES for the Western Portal position:

- Option A (**Concept Design**) - Commence the track slew toward the Melbourne Metro tunnels on the east side of Kensington Road into a decline structure situated in the council reserve on the south side of Childers Street into the portal located at the western interface of the cut and cover section directly opposite Ormond Street with the TBM retrieval shaft (eastern end of the cut and cover structure) situated in Bakehouse Road on the east side of McClure Road.
- Option B (**Alternative Design**) - Commence the track slew toward the Melbourne Metro tracks located on a widened embankment on the west side of Kensington Rd, cross Kensington Road on a rail over road bridge, progress on a decline structure situated in the council reserve on the south side of Childers Street into the portal located at the western interface of the cut and cover section approximately 120m west of Ormond Street with the TBM retrieval shaft (eastern end of the cut and cover structure) situated within the council reserve immediately west of the existing South Kensington station subway at the intersection of Ormond and Childers Street.

(SOURCE: Environmental Effects Statement Chapter 5 Project Development).

3 Pedestrian Access

Pedestrian access plans for both Western Portal options are provided in Appendix A: as follows.

- MMR-ASV-PMAA-M2-4261 – Option A - Pedestrian Routes
- MMR-ASV-PMAA-M2-4262 – Option B - Pedestrian Routes
- MMR-ASV-PMAA-M2-4263 – Option B – Pedestrian Underpass Detail
- MMR-ASV-PMAA-M2-4264 – Option A – Pedestrian Underpass Section – Stage 2
- MMR-ASV-PMAA-M2-4266 – Option A – Pedestrian Underpass Detail - Stage 1
- MMR-ASV-PMAA-M2-4267 – Option A – Pedestrian Underpass Detail - Stage 2

South Kensington Station Access

Access to South Kensington Station pedestrian underpass would be maintained at all times for both options except for major rail occupations that include the Werribee Line, when the station would be closed.

Option A requires a cut and cover box to be constructed along Childers Street between Ormond Street and the TBM Shaft at the 50 Lloyd Street Business Estate. Pedestrian access to South Kensington Station would be maintained at all times during this construction. The station access would be clearly signed and hoarded off at all times allowing unobstructed access by pedestrians wishing to commence or terminate a train journey at this location.



The Childers Street cut and cover box will be constructed in two stages to facilitate access to South Kensington Station. The routing of pedestrian access would vary across the site as construction progresses. The work would progress as follows:

- Stage 1 – Construct the piles and roof of the cut and cover structure along Childers Street to the west and east of South Kensington Pedestrian Underpass leaving an uncompleted section of approximately 10m width directly in front of the underpass.
- Stage 2 - Complete the section of piles and roof structure in front of the South Kensington Station pedestrian underpass with pedestrian access maintained and rerouted over the completed structure to the east.

For Option B the works do not impact the existing pedestrian underpass.



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Appendix A: Pedestrian Access Plans





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OPTION A - CONCEPT DESIGN

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 26/08/2016

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MELBOURNE METRO
WESTERN PORTAL
OPTION A
PEDESTRIAN ROUTES

Up Location East. North. ID#	Down Location East. North. ID#	Datum MGA Z55
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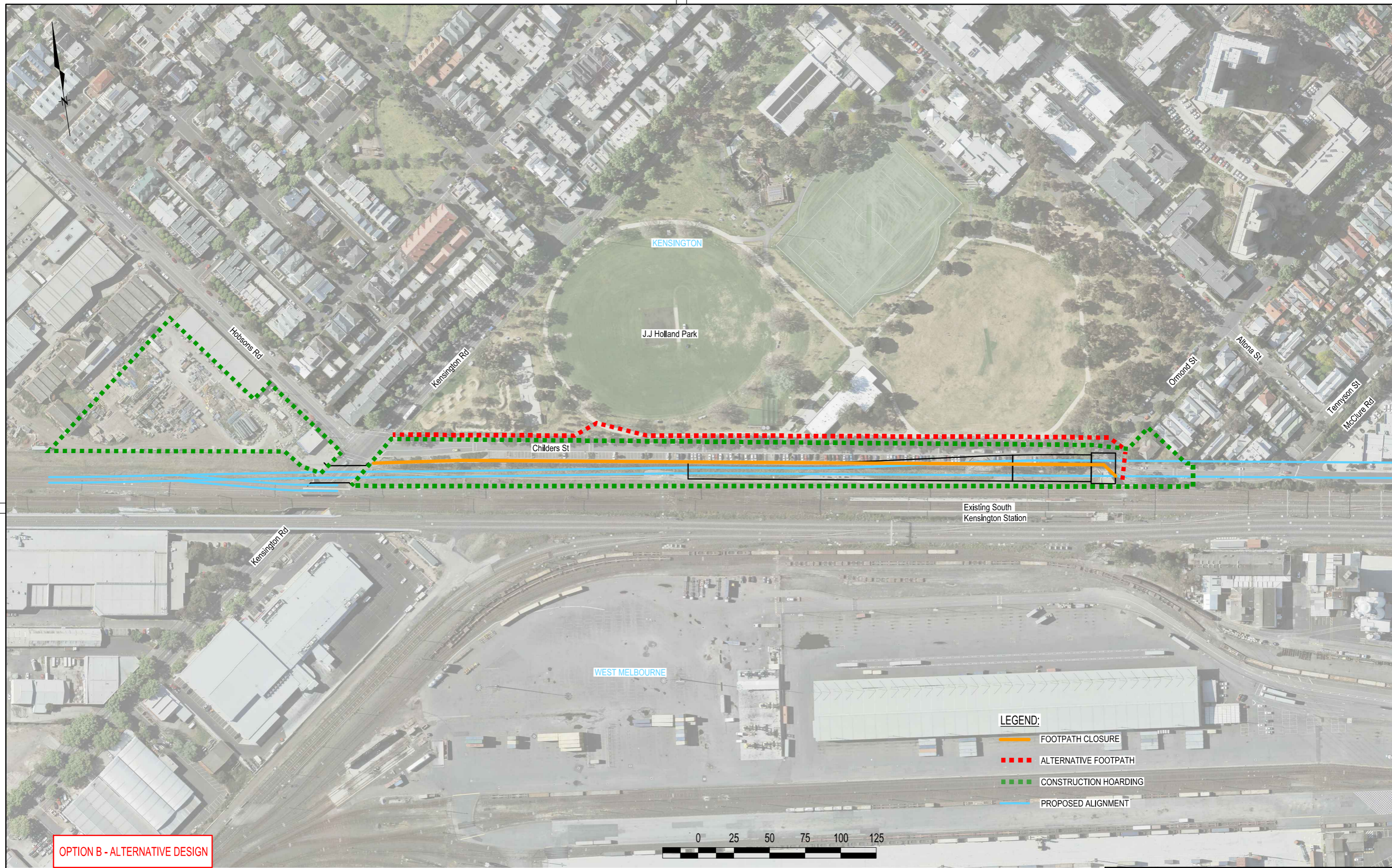
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Drawing Number	Revision 1.1

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OPTION B - ALTERNATIVE DESIGN

- LEGEND:**
- FOOTPATH CLOSURE
 - - - ALTERNATIVE FOOTPATH
 - - - CONSTRUCTION HOARDING
 - PROPOSED ALIGNMENT

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 30/08/2016

Revised By	In Serv	Rev.	Date	IAC PLAN	Description	Designed	Checked	Ind. Rev.	Approv.
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MELBOURNE METRO
WESTERN PORTAL
OPTION B
PEDESTRIAN ROUTES

Up Location East. North. ID#	Down Location East. North. ID#	Datum MGA Z55
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Project Drawing Number MMR-ADV-PMAA-M2-DD-4262.dgn



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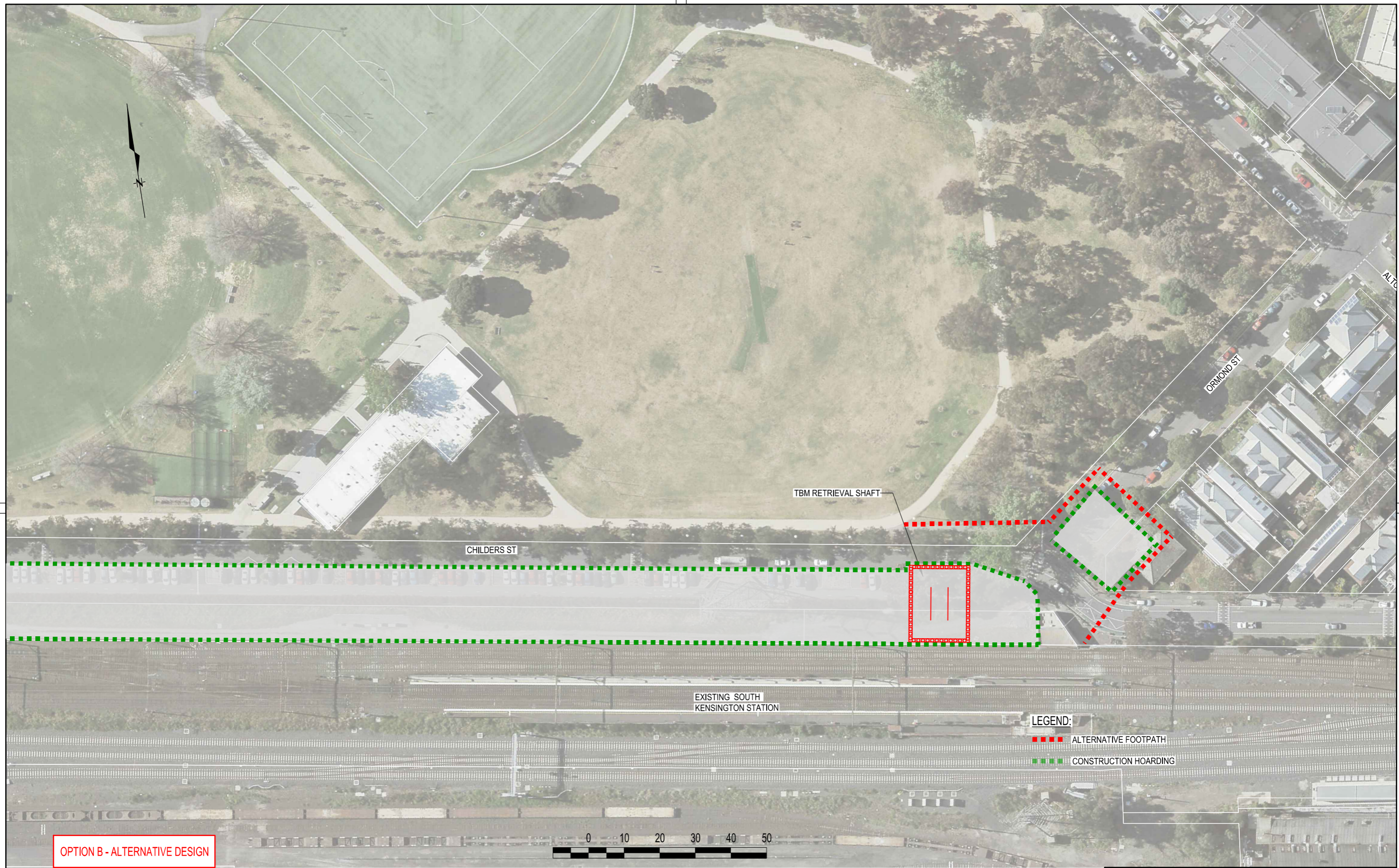
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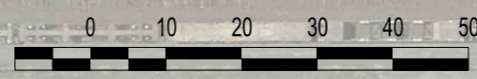
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OPTION B - ALTERNATIVE DESIGN

LEGEND:
 ■■■■ ALTERNATIVE FOOTPATH
 ■■■■ CONSTRUCTION HOARDING



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26/08/2016

Revised By	In Serv	Rev.	Date	Description	Designed	Checked	Ind. Rev.	Approv.
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MELBOURNE METRO
WESTERN PORTAL
OPTION B
PEDESTRIAN UNDERPASS DETAIL

Up Location East. North. ID#	Down Location East. North. ID#	Datum MGA Z55
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Project Drawing Number MMR-ADV-PMAA-M2-DD-4263.dgn

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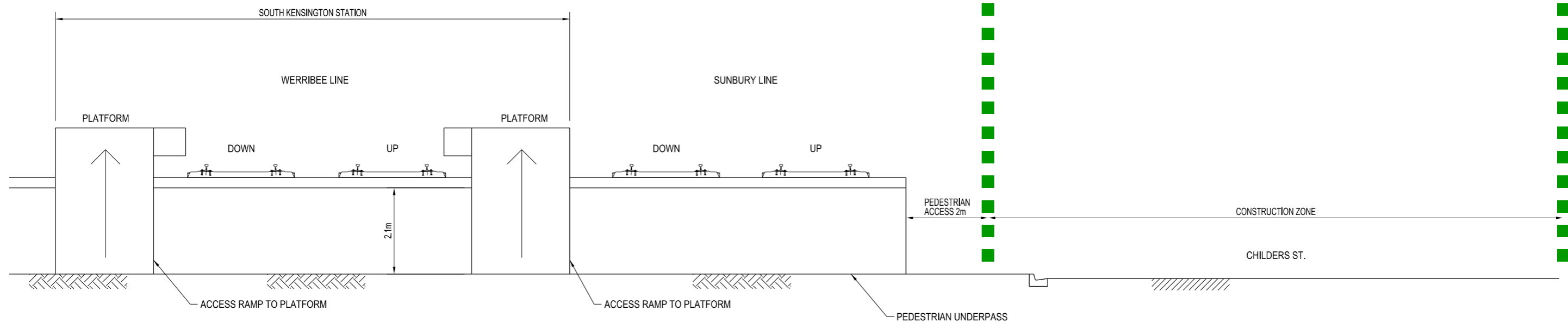
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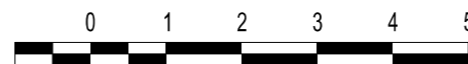
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TYPICAL SECTION THROUGH PEDESTRIAN UNDERPASS

NOTE: SUNBURY LINE TRACKS ARE REINSTATED ON EXISTING UNDERPASS STRUCTURE AT EXISTING LEVELS.

OPTION A - CONCEPT DESIGN



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MELBOURNE METRO
WESTERN PORTAL
OPTION A
PEDESTRIAN UNDERPASS SECTION - STAGE 2

Up Location East. North. ID#	Down Location East. North. ID#	Datum MGA Z55
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Project Drawing Number MMR-ADV-PMAA-M2-DD-4264.dgn

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- LEGEND:**
- ALTERNATIVE FOOTPATH
 - CONSTRUCTION HOARDING

OPTION A - CONCEPT DESIGN

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MELBOURNE METRO
WESTERN PORTAL
OPTION A
PEDESTRIAN UNDERPASS DETAIL - STAGE 1

Up Location	Down Location	Datum
East. North. ID#	East. North. ID#	MGA Z55

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File Name

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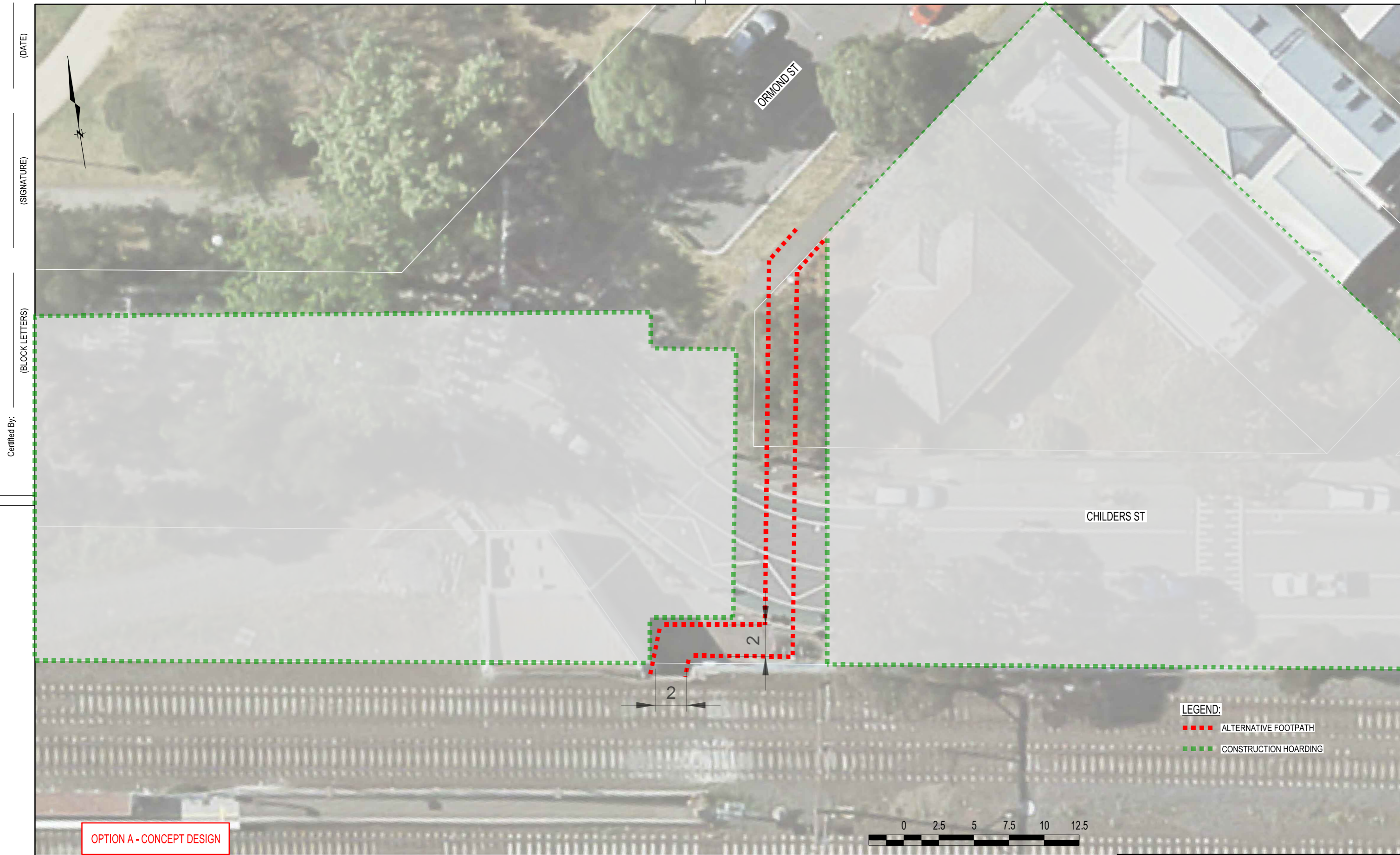
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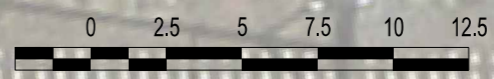
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- LEGEND:**
- - - - ALTERNATIVE FOOTPATH
 - - - - CONSTRUCTION HOARDING

OPTION A - CONCEPT DESIGN



11:25:01 AM
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MELBOURNE METRO
WESTERN PORTAL
OPTION A
PEDESTRIAN UNDERPASS DETAIL - STAGE 2

Up Location	Down Location	Datum
East. North. ID#	East. North. ID#	MGA Z55

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