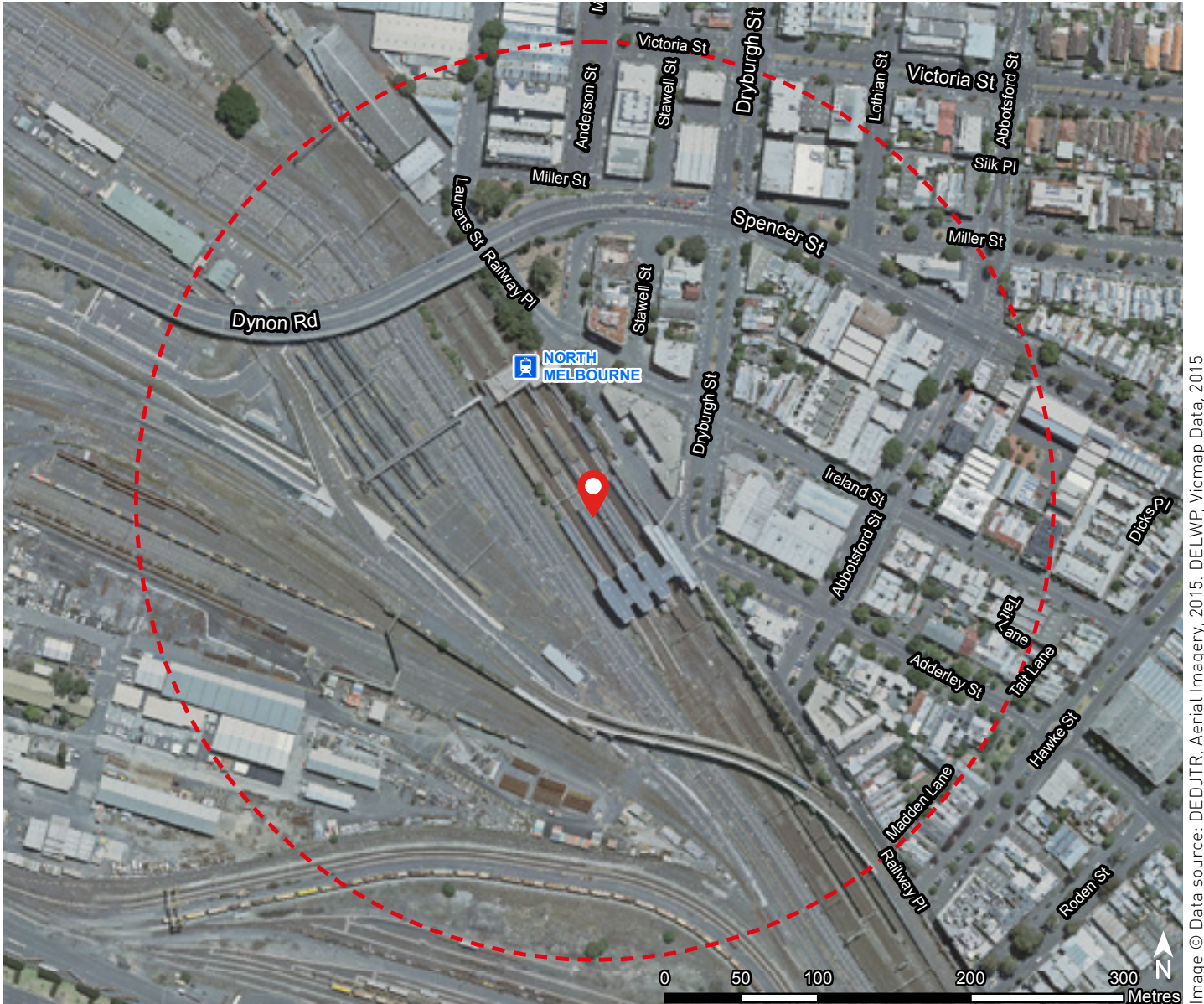


# North Melbourne Station

## Melbourne



North Melbourne Station opened in 1859 and is a major interchange station servicing the Craigieburn, Flemington Racecourse, Sunbury, Upfield, Werribee and Williamstown lines. The premium station has undergone various expansions and renovations over the years.

The station was recently redeveloped in 2009 which included a new main entrance at the southern end of the station with escalators, stairs and lifts installed.

Image © Data source: DEDJTR, Aerial Imagery, 2015. DELWP, Vicmap Data, 2015



Figure M 49 North Melbourne Station platforms



Figure M 50 Pedestrian overpass



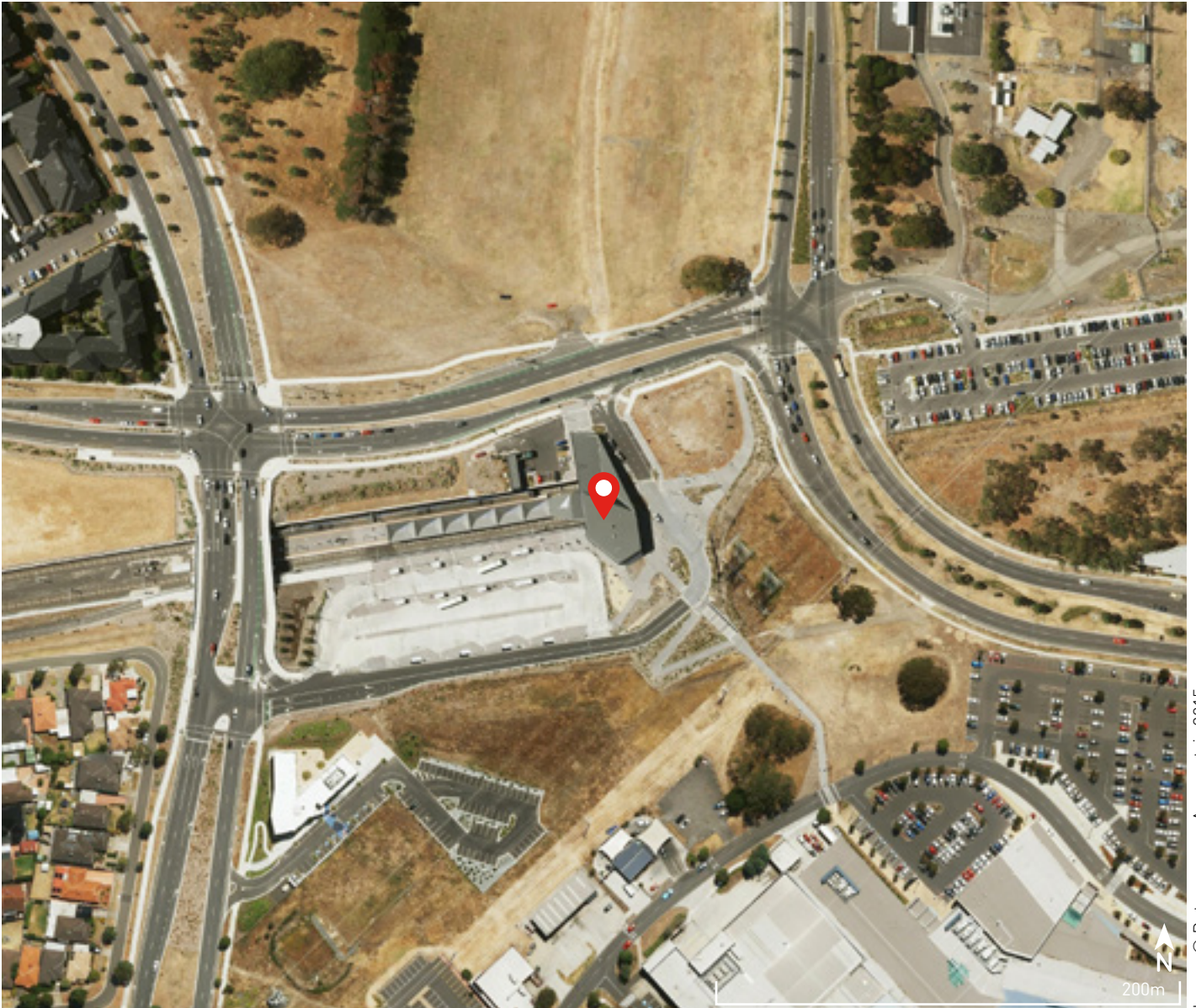
Figure M 51 Escalators



Figure M 52 Platforms

# South Morang Station

## Melbourne



South Morang station opened in April in 2012 and is the current terminus of the South Morang line. It is an example of a rail under station where the rail corridor runs under Civic Dr. The station is clad in steel panels, features a timber underside and is prominently sited.

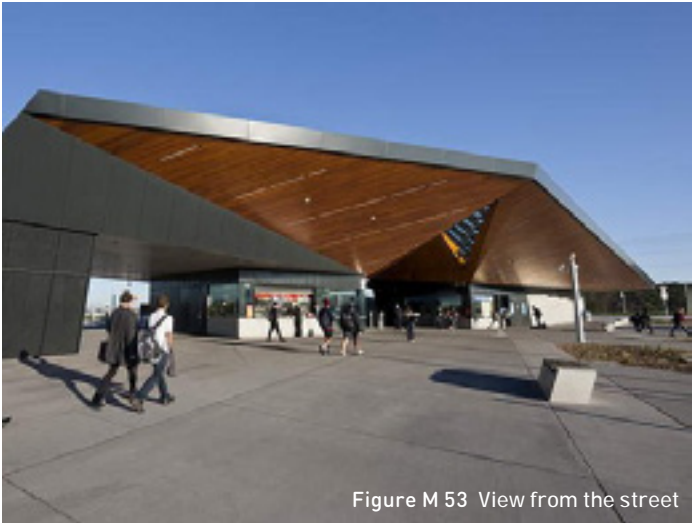


Figure M 53 View from the street



Figure M 54 Platforms



Figure M 55 Staircase to platforms



Figure M 56 Platform



Figure M 57 View from the street

# Warrigal Road, Oakleigh Melbourne

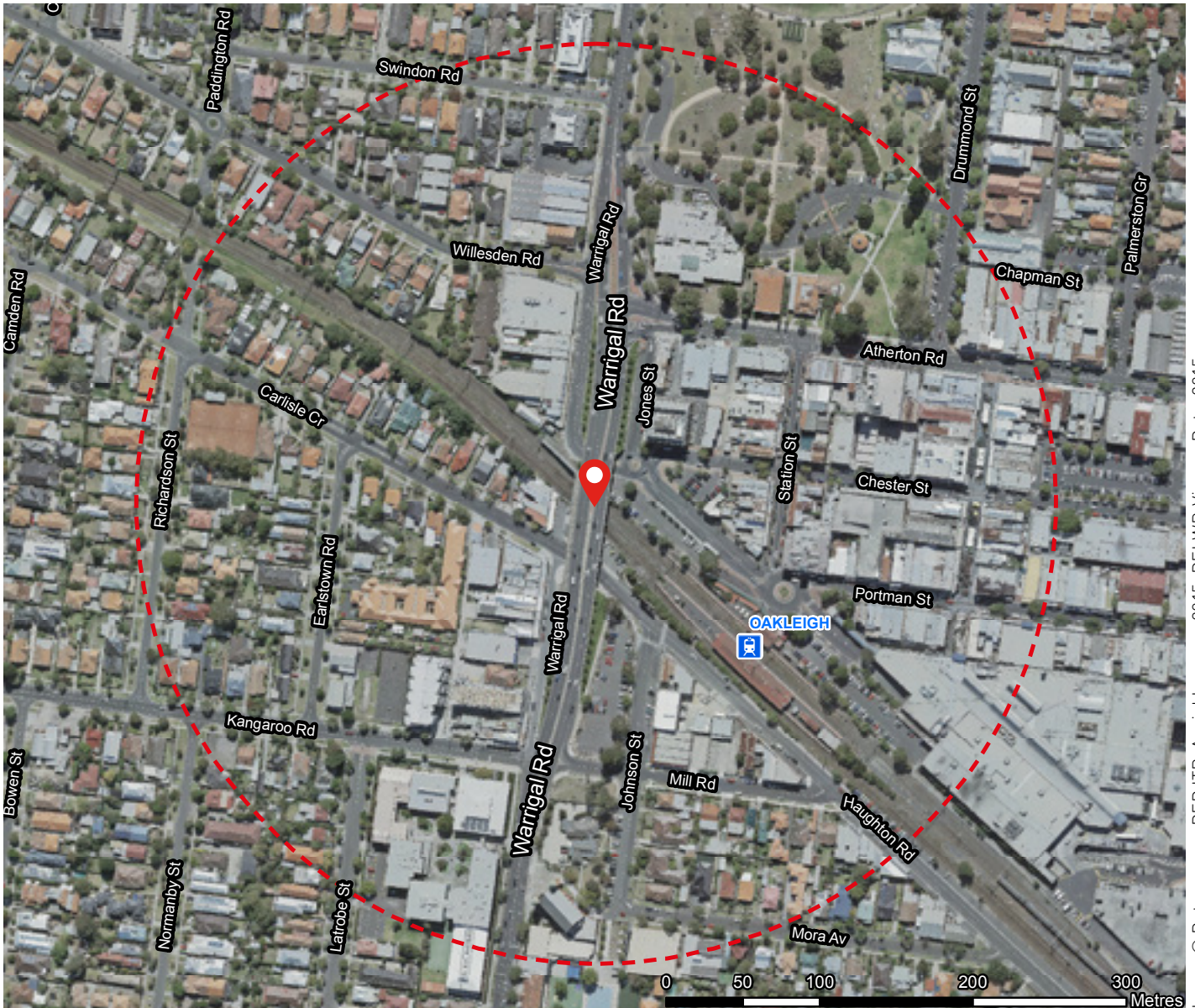


Image © Data source: DEDJTR, Aerial Imagery, 2015. DELWP, Vicmap Data, 2015

The elevated Warrigal Road which bridges over the Pakenham rail line is an example of a road over rail grade separation solution.



Figure M 58 View of at grade section of Warrigal Road and retaining wall of elevated section of Warrigal Rd looking north



Figure M 59 Pedestrian ramp which provides access up to Warrigal Rd from Burlington St



Figure M 60 View under Warrigal Rd from Burlington St looking east

# EastLink

## Melbourne



Image © Data source: Terramatrix 2016 & Aerometrix 2016

The EastLink toll road is 39 km section of the Eastern Freeway (M3) which connects to the Monash, Frankston and Peninsula Link freeways.

The urban design of the tollway includes a strong colour palette and road art.

The artistic and architectural design is evident in the pedestrian overpasses, bridge balustrades and noise attenuation structures.

The extensive road landscaping was the largest landscaping program ever undertaken on any road project in Australia. Approximately 4 million native plants, shrubs and trees were planted in the EastLink landscape.

The EastLink trail provides pedestrians and cyclists with a shared 3m concrete path that follows the part of the route.



Figure M 61 Composite noise wall



Figure M 62 Eastlink trail



Figure M 63 Eastlink trail



Figure M 64 Eastlink



Figure M 65 Eastlink tunnel



Figure M 66 Eastlink overpasses





Figure M 67 Landscaping



Figure M 68 Overpass



Figure M 69 Artwork



Figure M 70 Pedestrian bridge



Figure M 71 Noise wall



Figure M 72 Eastlink



Figure M 73 Eastlink



Figure M 74 Surrounding landscape



Figure M 75 Artwork



Figure M 76 Eastlink

# Peninsula Link

## Melbourne



Image © Data source: Terramatrix 2015 & Aeromatrix 2015 & Sinclair Knight Merz & Fugro 2015

Peninsula Link is a 27 km road link between Eastlink and the Mornington Peninsula Freeway. The road features an array of permanent and temporary art installations. A partnership with Southern Way and McClelland Gallery & Sculpture Park sees semi-permanent sculptures commissioned every 2 years.

The landscape architecture concept was influenced by the need to maximize natural plant regeneration.

The Peninsula Link shared use path contributes to the Frankston and Mornington Peninsula walking and cycling networks.

Signature elements incorporated into individual bridge colour palettes indicate the bridge's status along the freeway. For example a blue palette indicates a key intersection in the network and yellow indicates a secondary crossing. The design features aim to enhance legibility of the road and its location relative to the wider road network.



Figure M 77 Pedestrian bridge



Figure M 78 Noise walls



Figure M 79 Noise wall detail and viaduct



Figure M 80 Peninsula Link



Figure M 81 Noise wall detail and viaduct



Figure M 82 The King is Dead sculpture



Figure M 83 Peninsula Link



Figure M 84 Noise wall



Figure M 85 Peninsula Link



Figure M 86 Peninsula Link



Figure M 87 Noise wall



Figure M 88 Peninsula Link



Figure M 89 Operations and maintenance centre



Figure M 90 Peninsula Link

# CityLink

## Melbourne

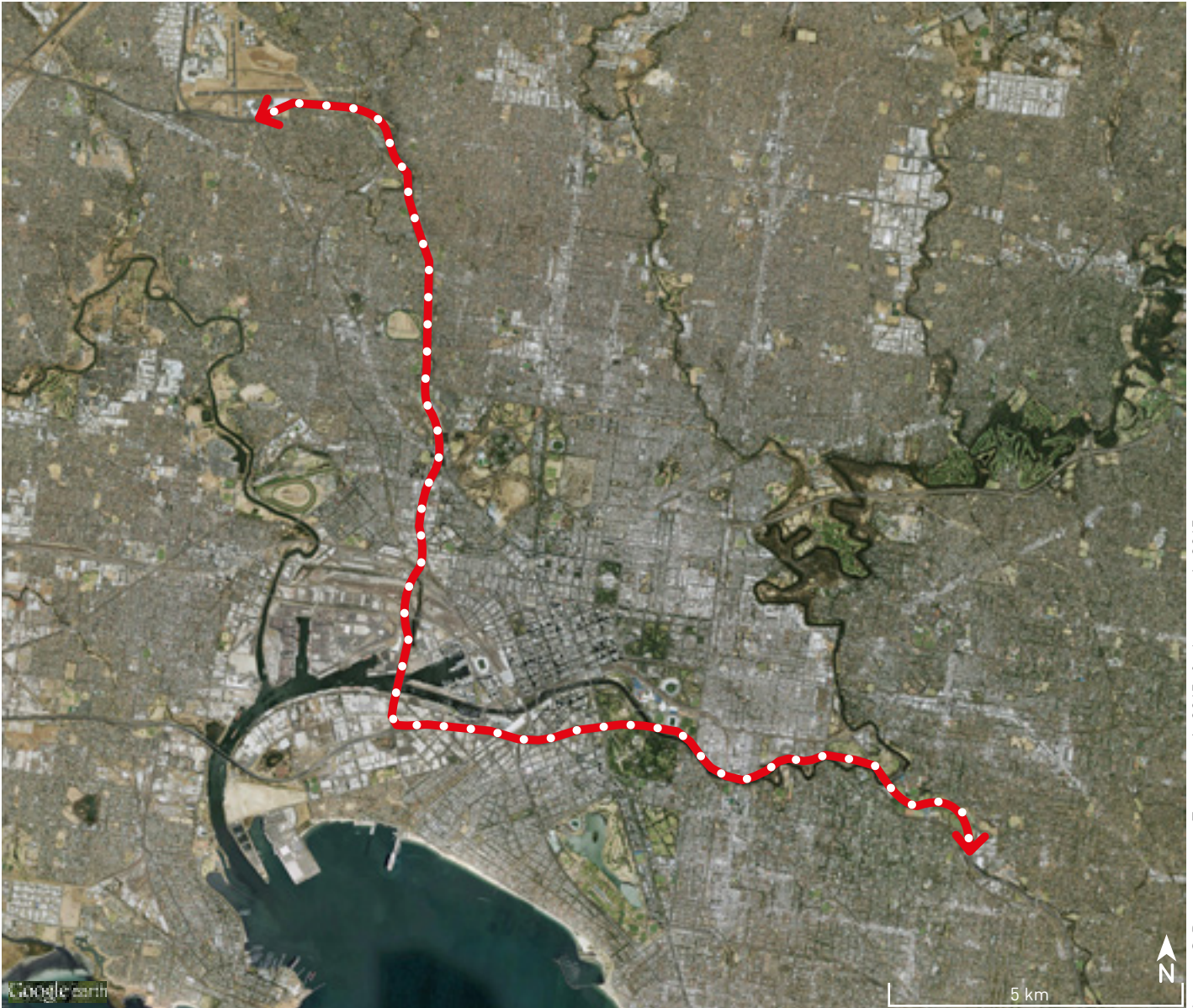


Image © Data source: Terramatrix 2015 & Aerometrix 2015

CityLink was built between 1996 and 2000 and is a network of tolled urban motorways in Melbourne, linking the West Gate, Tullamarine and Monash freeways. It incorporates the Bolte Bridge, Burnley Tunnel and provides a continuous, high-capacity road route to, and around, Melbourne's central business district.

Public art and gateway features include the iconic tube-like sound barrier in Flemington and a sculptural work called the Melbourne International Gateway.

The network includes provision for cyclists; however cyclists are excluded from riding the West Gate Freeway, the Bolte Bridge and over the West Gate Bridge.

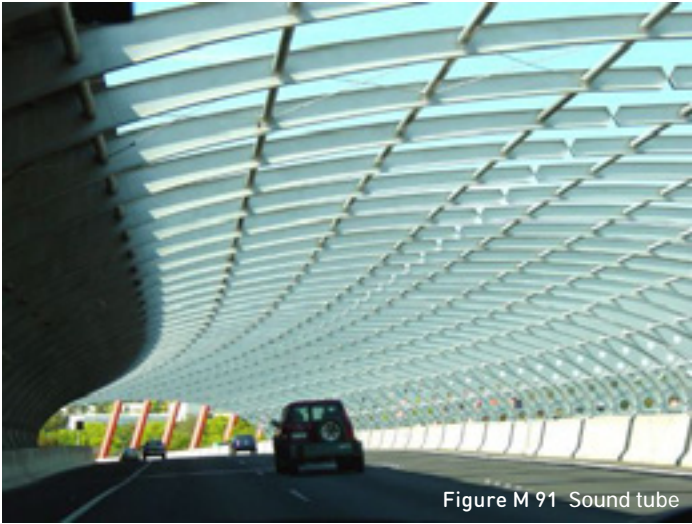


Figure M 91 Sound tube



Figure M 92 Sound tube



Figure M 93 Sound tube



Figure M 94 Gateway features



Figure M 95 Gateway feature



# Jim Stynes Bridge

## Melbourne

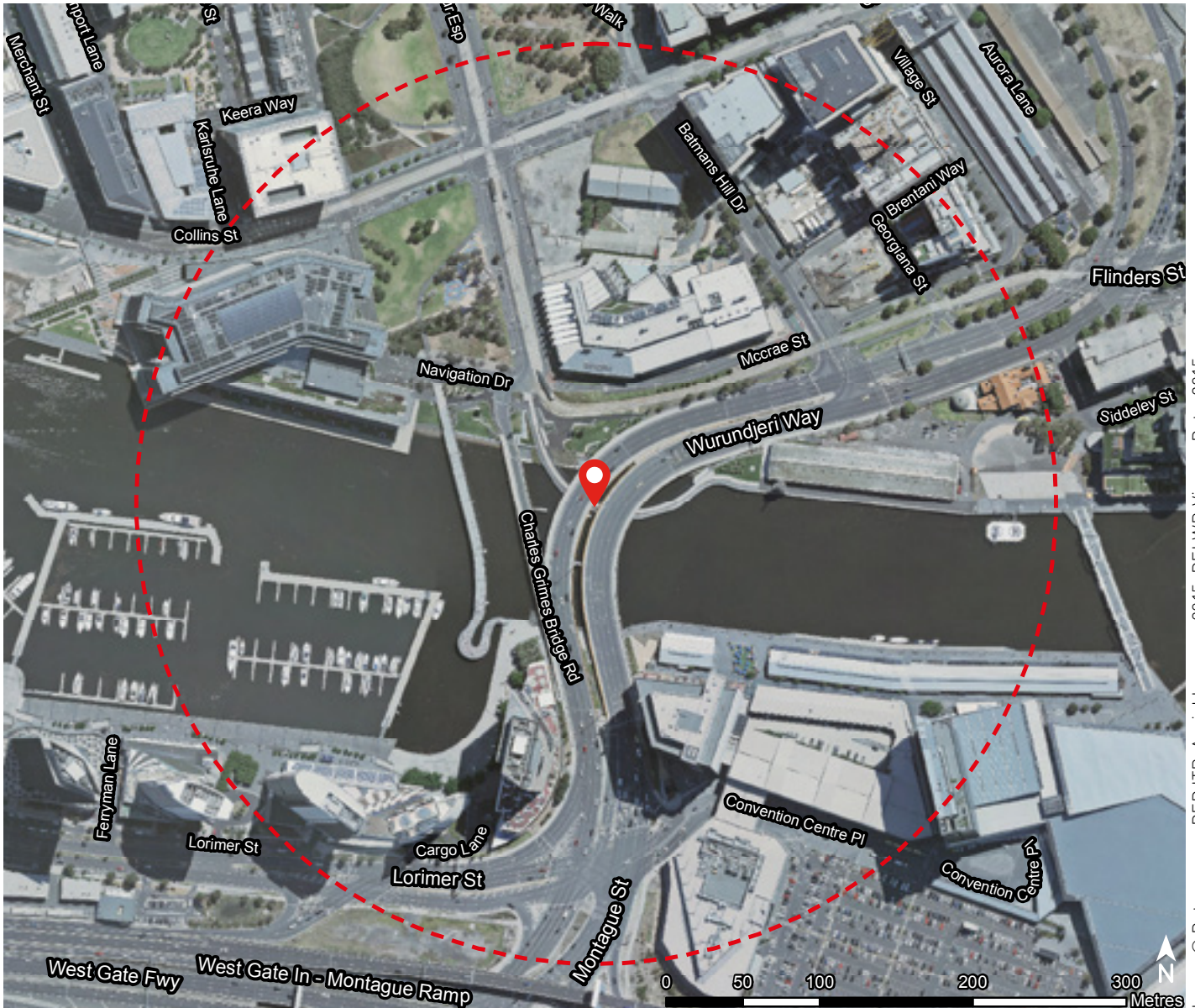


Image © Data source: DEDJTR, Aerial Imagery, 2015. DELWP, Vicmap Data, 2015

The Jim Stynes Bridge in Melbourne opened in 2014 in honour of the late footballer. The 120-metre-long horizontal suspension bridge arcs out 30 metres over the river.

This pedestrian and bicycle bridge cost \$15 million and provides a link for pedestrians, cyclists and commuters between Melbourne's CBD, and the key precincts of Docklands and Northbank.



Figure M 96 Jim Stynes Bridge



Figure M 97 Jim Stynes Bridge



Figure M 98 Jim Stynes Bridge



Figure M 99 Jim Stynes Bridge

