

MAJOR ROAD PROJECTS VICTORIA

JULY 2020

TECHNICAL REPORT H – PLANNING AND LAND USE IMPACT ASSESSMENT

Report No: 2135645A-N-32-PLA-REP-0001 REV00

YAN YEAN ROAD UPGRADE – STAGE 2: KURRAK ROAD TO BRIDGE INN ROAD

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Technical Report H – Planning and Land Use Impact Assessment Yan Yean Road Upgrade – Stage 2: Kurrak Road to Bridge Inn Road

Major Road Projects Victoria

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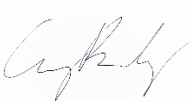


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ABBREVIATIONS

AH Act	<i>Aboriginal Heritage Act 2006</i>
CEMP	Construction Environmental Management Plan
CHMP	Cultural Heritage Management Plan
CL Reserves Act	<i>Crown Land (Reserves) Act 1978</i>
CoT	Certificates of Title
DCP	Development Contribution Plan
DELWP	Department of Environment Land Water and Planning
DoT	Department of Transport
EE Act	<i>Environment Effects Act 1978</i>
EES	Environment Effects Statement
EMF	Environmental Management Framework
EPBC Act	<i>Environment Protection and Biodiversity Conservation Act 1999 (Cth)</i>
EPR	Environmental Performance Requirement
ERA	Environmental Risk Assessment
ESO	Environmental Significance Overlay
HO	Heritage Overlay
LA&C Act	<i>Land Acquisition and Compensation Act 1986</i>
LPPF	Local Planning Policy Framework
LSP	Local Structure Plan
MAC	Metropolitan Activity Centre
MNES	Matters of National Environmental Significance
MREP	Mernda Rail Extension Project
MRPV	Major Road Projects Victoria
MSP	<i>Mernda Strategy Plan 2004 (Amended 2016)</i>
MSS	Municipal Strategic Statement
MTC	Mernda Town Centre
MTIA	Major Transport Infrastructure Authority
MTPFA	<i>Major Transport Projects Facilitation Act 2009</i>
MW	Melbourne Water
NEIC	National Employment and Innovation Cluster

NPS	Nillumbik Planning Scheme
NRMP	<i>Nillumbik Roadside Management Plan 2012</i>
NSC	Nillumbik Shire Council
P&E Act	<i>Planning and Environment Act 1987</i>
PAO	Public Acquisition Overlay
PBN	Principal Bicycle Network
PCRZ	Public Conservation and Resource Zone
PGPR	Plenty Gorge Park Reserve
PPF	Planning Policy Framework
PSA	Planning Scheme Amendment
RCZ	Rural Conservation Zone
RDA	Regional Development Australia
RDZ	Road Zone
RM Act	<i>Road Management Act 2004</i>
SCO	Specific Controls Overlay
SUP	Shared Use Path
The Project	The Yan Yean Road Upgrade – Stage 2
TI Act	<i>Transport Integration Act 2010</i>
TRG	Technical Reference Group
VIF	<i>Victoria in Future 2016</i>
VPO	Vegetation Protection Overlay
VPPs	Victoria Planning Provisions
WCC	Whittlesea City Council
WPS	Whittlesea Planning Scheme
YVW	Yarra Valley Water

EXECUTIVE SUMMARY

This report comprises Technical Report H to the Yan Yean Road Upgrade – Stage 2 (the Project) Environment Effects Statement (EES). It has been used to inform the EES required for the Project. The report addresses the potential impacts to planning and land use resulting from the construction and operation of the Project.

The Project is being delivered by Major Road Projects Victoria (MRPV). Once the Project is completed, the operation and maintenance responsibilities will be handed over to Department of Transport (DoT) and/or the relevant local government authority.

PROJECT OVERVIEW

Melbourne is experiencing rapid population growth, a changing economic geography and an expanding freight task, particularly in the western, northern and south-eastern outer suburbs. The suburbs of Mernda and Doreen within the North Growth Corridor are experiencing unprecedented levels of population growth resulting in rapid land use change and growing travel demand. The resultant urban development and population growth has outpaced the capacity of the arterial road network and in recognition of this, the Commonwealth and State Governments are investing in significant upgrades to transport infrastructure within the outer suburbs.

Yan Yean Road is a secondary north-south arterial connection located within the City of Whittlesea and the Shire of Nillumbik and connects Doreen, Yarrambat and Plenty. It also provides connectivity to east-west arterials including Bridge Inn Road, Kurrak Road and Diamond Creek Road linking residents to the suburbs of Mernda, South Morang, Diamond Creek and Greensborough which provide regional employment and services and access to the metropolitan rail network. Yan Yean Road partially forms the boundary between the City of Whittlesea and Nillumbik Shire, to the north of Jorgensen Avenue, Doreen and is subject to the policies and requirements of the Nillumbik Planning Scheme (NPS) and Whittlesea Planning Scheme (WPS).

The Project proposes to duplicate 5.5 kilometres of Yan Yean Road from two lanes to four lanes, upgrade intersections and provide a continuous shared use path (SUP) along the western side of the Project extent. This Project will complete the full duplication between Diamond Creek and Doreen.

EXISTING CONDITIONS

The study area for the purpose of this planning and land use assessment consists of all land located within the project boundary plus land within a 200 metre buffer directly abutting this boundary to enable key land uses within the broader vicinity of the Project to be captured.

The Project is located within a predominantly low density residential and rural living area within the metropolitan Green Wedge. The north-west end of Yan Yean Road forms the eastern edge of the Northern Growth Corridor which is experiencing rapid land use and character change from rural living to residential and is subject to the land use and development framework established by the *Mernda Strategy Plan 2004* (Amended 2016) (MSP). Residential land uses in the area north of Jorgensen Avenue and to the west of Yan Yean Road are characterised by conventional residential blocks.

Within the Green Wedge, rural land consists of rural residences set in a natural environment interspersed with small scale agricultural farms. Blocks are large with built form setback from the road and located within large landscaped gardens containing border trees. The settlement pattern of the township of Yarrambat is predominately larger rural residential lots established by the *Yarrambat Local Structure Plan, 1993* (Revised 2000). The Plan formalised a landscape reserve along the frontage of land abutting Yan Yean Road to restrict access, as well as requiring a 30 metre building setback along the road to maintain the rural character and preserve the opportunity for any road widening required in the longer term. This is secured by the requirement to enter into section 173 agreements with Nillumbik Shire Council (NSC).

The study area also includes: education uses with Plenty Valley Christian College and Yarrambat Primary School abutting the project boundary; recreation and open space at Doreen Recreation Reserve, Werther Park, Yarrambat Park and Plenty Gorge Parkland Reserve (PGPR); and commercial premises interspersed along the alignment that are accessed directly via Yean Yan Road or via adjacent access roads.

RISK ASSESSMENT

The EES requires technical documentation be prepared using a risk-based approach to impact assessment so that a greater level of effort is directed at investigating and addressing those matters that pose a relatively higher risk of adverse effects. The Environmental Risk Assessment (ERA) prepared for the Project identified six risks for planning and land use with initial risk ratings of medium or higher. These risks pertain to land use, land acquisition, severance and occupation. Following the implementation of the standard controls and Environmental Performance Requirements (EPRs), all likelihoods of risks were reassessed, resulting in reduced residual risk ratings.

IMPACT ASSESSMENT

The Project requires the acquisition of private, public and council land. The reference design has evolved in consideration of the need to avoid and minimise impacts on the topography, built form, land use, land acquisition and vegetation/habitat of high biodiversity value.

During construction, the road reservation will be used for temporary works and laydown areas; relocation and construction of utility installations and traffic diversions. The occupancy will be temporary and alternate access will be made available. Land use impacts during construction can be managed through appropriate mitigation measures such as provision of alternative access and regular communication with road users and affected landowners.

The widening of Yan Yean Road has been identified in strategic planning policy since the *Plenty Valley Strategic Plan 1990* and land has been set aside through the *Yarrambat Local Structure Plan* for future widening. The land use of the Project is generally consistent with and implements State and local planning policy and the strategic framework of the WPS and NPS.

At the time of writing this report, the Project requires partial or full acquisition of 96 parcels, which will result in permanent reduction in land area. These parcels are not currently subject to a PAO. For parcels without existing inter-government acquisition arrangements, a PAO would be required to be applied to the land under the *Land Acquisition and Compensation Act 1986* (LA&C Act). Of the land to be acquired that is not currently subject to a PAO, ownership details are as follows:

Table ES.1 Proposed land acquisition for the Project

LANDOWNER STATUS	NO. OF PARCELS
Council	28 (26 NSC and 2 WCC)
Private	60
Public Authorities/State	8

The key land use impact can be summarised as removal of built form, reduction in the area of land for open space, changes to access and amenity. In most cases, partial acquisition of the land would be required along the frontage of landholdings, which are limited in extent, and will not result in long term land use change. The key impacts include:

- Acquisition of land along the frontage 590 Yan Yean Road. This property contains an unoccupied dwelling and an additional new dwelling. The Project will result in the demolition of the unoccupied dwelling and subsequently result in a change in land use. However, it is a condition of the planning permit that this residential building be removed. Therefore, the removal of this structure is a requirement of the active planning permits rather than as a result of this Project. The new residential building will not be impacted by the Project and the parcel will be retained for residential purposes and no residents will be displaced.
- Potential inconsistency with existing planning policy documents (*Doreen Recreational Reserve Masterplan 2014*, *Development Plan 815–835 Yan Yean Road, 2017* and the *Mernda Strategy DCP 2008*) and loss of viable open space at the Doreen Recreation Reserve. Minor reduction in open space with partial acquisition of land from PGPR and Yarrambat Park to facilitate the Project.
- Acquisition resulting in permanent access changes to residential, education and commercial land uses.

The assessment further identifies the land use benefits of the Project. The Project provides a continuous SUP along the western side of Yan Yean Road and a separated footpath along the east. This will deliver improved active transport links along the Principal Bicycle Network (PBN) to key land uses and contribute to strategic and local policy objectives. The road will also improve the capacity and connectivity of the road network in the outer suburbs in line with the outcomes, directions and policies of the Metropolitan Planning Strategy *Plan Melbourne 2017–2050* (Plan Melbourne).

During construction and operation there is potential for amenity impacts including disruption to access, increase in construction traffic, noise, air quality and visual impacts associated with the Project. Amenity impacts are often interrelated, and any mitigation measures have been assessed more specifically within the relevant supporting EES technical reports.

Land use impacts have been considered in the context of the identified risk pathways. Overall, the key land use impacts will be managed in accordance with the Environmental Management Framework (EMF) for the Project. Impacts to amenity will be managed through Environmental Performance Requirements (EPRs) developed by relevant associate environmental disciplines (including environmental management, transport, contaminated land, landscape and visual, noise, air quality, biodiversity).

With the implementation of standard and additional controls detailed within Section 7, and specific EPRs in Section 8, the construction and operational impacts to planning and land use are considered adequately managed.

Land use impacts have been considered in the context of the identified risk pathways. Overall, the key land use impacts will be managed in accordance with the EMF for the Project. Impacts to amenity will be managed through EPRs developed by relevant associate environmental disciplines (including environmental management, transport, contaminated land, landscape and visual, noise, air quality, biodiversity).

PLANNING PATHWAY

The proposed planning pathway for the use and development associated with this Project is via a Planning Scheme Amendment (PSA) to the NPS and WPS. Amendment GC92 would amend the NPS and WPS to apply Clause 45.12 Specific Controls Overlay and Clause 45.01 Public Acquisition Overlay to affected land, and insert the *Yan Yean Road (Kurrag Road to Bridge Inn Road) (Stage 2) Upgrade Project Incorporated Document* into the Schedules to Clause 45.12 and Clause 72.04. This would facilitate the use and development of the land for the Project subject to the specific controls outlined in the Incorporated Document.

In accordance with the *Ministerial Guidelines for Assessment of Environmental Effects under the EE Act (Ministerial Guidelines) 2006*, the draft Amendment GC92 would be exhibited concurrently with the EES and a joint Inquiry and Advisory Committee would be appointed to consider submissions on the draft PSA and EES. The Minister for Planning's assessment of the EES would inform the consideration of the draft PSA and the Commonwealth Minister for the Environment's approval decisions under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act).

1 INTRODUCTION

Major Road Projects Victoria (MRPV) proposes to duplicate Yan Yean Road between Kurrak Road to Bridge Inn Road as part of the Yan Yean Road Upgrade – Stage 2 (the Project).

On 14 October 2018, the Minister for Planning determined an Environment Effects Statement (EES) would be required under the *Environment Effects Act 1978* (EE Act) to assess the potential environmental effects of the Project. The EES process provides for identification and analysis of the potential environment effects of the Project and the means of avoiding, minimising and managing adverse effects. It includes public involvement and allows stakeholders to understand the likely environmental effects of the Project and how they will be managed.

This planning and land use assessment report has been prepared for the EES in accordance with the Scoping Requirements released by the Minister for Planning in June 2019.

1.1 BACKGROUND

Yan Yean Road is a primary north-south transportation corridor connecting the growth suburb of Doreen and the surrounding northern catchment, with major east-west arterials roads including Bridge Inn Road, Kurrak Road and Diamond Creek Road.

Stage 1 of the Yan Yean Road upgrade (Diamond Creek Road to Kurrak Road) was completed in 2019, and construction on Stage 2 (this Project) is to be completed by 2025, subject to planning approval.

The Project straddles the boundary of the Nillumbik and Whittlesea Local Government Areas.

1.2 PURPOSE

The need to upgrade Yan Yean Road to a secondary arterial road to support the development of the North Growth Corridor was identified as early as 1990 within the *Plenty Valley Strategic Plan 1990*. Planning for Mernda-Doreen identified the need to upgrade Yan Yean Road and the use and development framework for this area was established by the *Mernda Strategy Plan 2004* (Amended 2016). Subsequent strategic plans provided for a 30 metre building setback and restricted access to facilitate its future upgrade (*Yarrambat Local Structure Plan 1993* (Revised 2000)). These early decisions shaped the land use and settlement pattern within Doreen, Yarrambat and Plenty and established the need for this Project.

The Project seeks to upgrade an existing road sited largely within an established lower density residential area and at the edge of the northern growth corridor; which is experiencing substantial land use change. The Project alignment and immediate surrounds intersect a range of land uses including residential, open space, rural living, commercial and education. This report provides an assessment of the land use planning considerations associated with the construction and operation of the Project that would have the potential to impact existing and future land use, built form and the strategic land use planning policy framework.

The impact assessment and mitigation measures will inform the development of a robust Environmental Management Framework (EMF) for the Project and support Planning Scheme Amendment (PSA) GC92 (Amendment GC92). The EMF will outline the approach to managing and monitoring environmental effects and hazards associated with the construction and operational phases of the Project.

Other aspects closely related to land use and development include: transport connectivity, social, amenity, historic and Aboriginal cultural heritage, and landscape and visual impacts. These are addressed in the following technical reports:

- Technical Report A – Traffic Impact Assessment
- Technical Report D – Social Impact Assessment.
- Technical Report F – Aboriginal Cultural and Historic Heritage Impact Assessment
- Technical Report G – Landscape Strategy.

1.3 PROJECT NEED

Yan Yean Road is a secondary north-south arterial connection located approximately 25 kilometres north-east of Melbourne CBD within the City of Whittlesea and the Shire of Nillumbik and connects Doreen, Yarrambat and Plenty. It also provides connectivity to east-west arterials including Bridge Inn Road, Kurrak Road and Diamond Creek Road linking residents to the suburbs of Mernda, South Morang, Diamond Creek and Greensborough which provide regional employment and services and access to the metropolitan rail network. The arterial road network also provides access to critical employment clusters in the broader North Growth Corridor including Thomastown, Somerton and the La Trobe National Employment and Innovation Cluster (NEIC).

The suburbs of Mernda and Doreen within the North Growth Corridor are experiencing unprecedented levels of population growth resulting in rapid land use change and growing travel demand. The Mernda Rail Extension Project (MREP) was completed in August 2018 and strategic plans have been approved to guide the land use and development framework for the Mernda Town Centre (MTC). Population and urban growth has outpaced the capacity of the arterial road network and in recognition of this, the Commonwealth and State Governments are investing in significant upgrades within the outer suburbs.

Whittlesea City Council (WCC) and Nillumbik Shire Council (NSC) identify the lack of arterial road capacity and connectivity as a significant constraint to future growth. Both Councils have long been advocating for the upgrade. The *Yarrambat Extension Area Local Structure Plan, 1996* (Revised 2000) identifies capacity constraints on Yan Yean Road as a result of increasing through traffic. More recently, the *Yarrambat Issues and Options Paper 2014* highlights that local residents must travel to meet their retail and service needs and consequently, Yan Yean Road carries a high level of traffic from growth areas to the north (Doreen) and with sub-standard turning lanes, creates safety and access issues. Similarly, the City of Whittlesea produced the *Road and Public Transport Plan 2017* to identify priorities for transport infrastructure to meet with the needs of the rapidly expanding municipality. The Plan identifies the duplication of Yan Yean Road, from Kurrak Road to Bridge Inn Road, and enhancements to Yan Yean Road north of Bridge Inn Road, as a very high priority.

1.4 PROJECT DESCRIPTION

The Project would duplicate a 5.5 km portion of Yan Yean Road between Kurrak Road and Bridge Inn Road increasing the existing two lanes to four lanes (comprising two lanes in each direction). The design speed along Yan Yean Road is 70 km/h, with the exception of north of Bridge Inn Road which is 80 km/h. The design for the Project has 3.5 metre wide lanes with the majority of the Project using a 2.2 metre-wide central median. This cross section was adopted in design due to various constraints ranging from road safety issues, steep and rolling terrain, high cut and fill batters and subsequent retaining walls at certain locations, as well as seeking to limit impacts to existing properties, local accesses and trees along Yan Yean Road.

The Project will include:

- two new roundabouts (at Heard Avenue and Youngs Road)
- five new signalised intersections (Bannons Lane, Jorgensen Avenue, North Oatlands, Orchard and Bridge Inn Roads)
- upgrades to one existing signalised intersection, including an additional right hand turning lane, slip lane, and traffic island (Ironbark Road)
- new street lighting at all intersections, road signage and landscaping.

The Project will also include a new 3 metre-wide shared user path on the western side and 1.2 metre-wide footpath on the eastern side of Yan Yean Road. The paths links Diamond Creek to Doreen and would improve safety and connectivity for pedestrians and cyclists.

Continuous safety barriers would run along the Project's length and are proposed in the median and behind outer kerbs along the mid-block sections of the carriageways.

The project area and key Project components are shown in Figure 1.1.

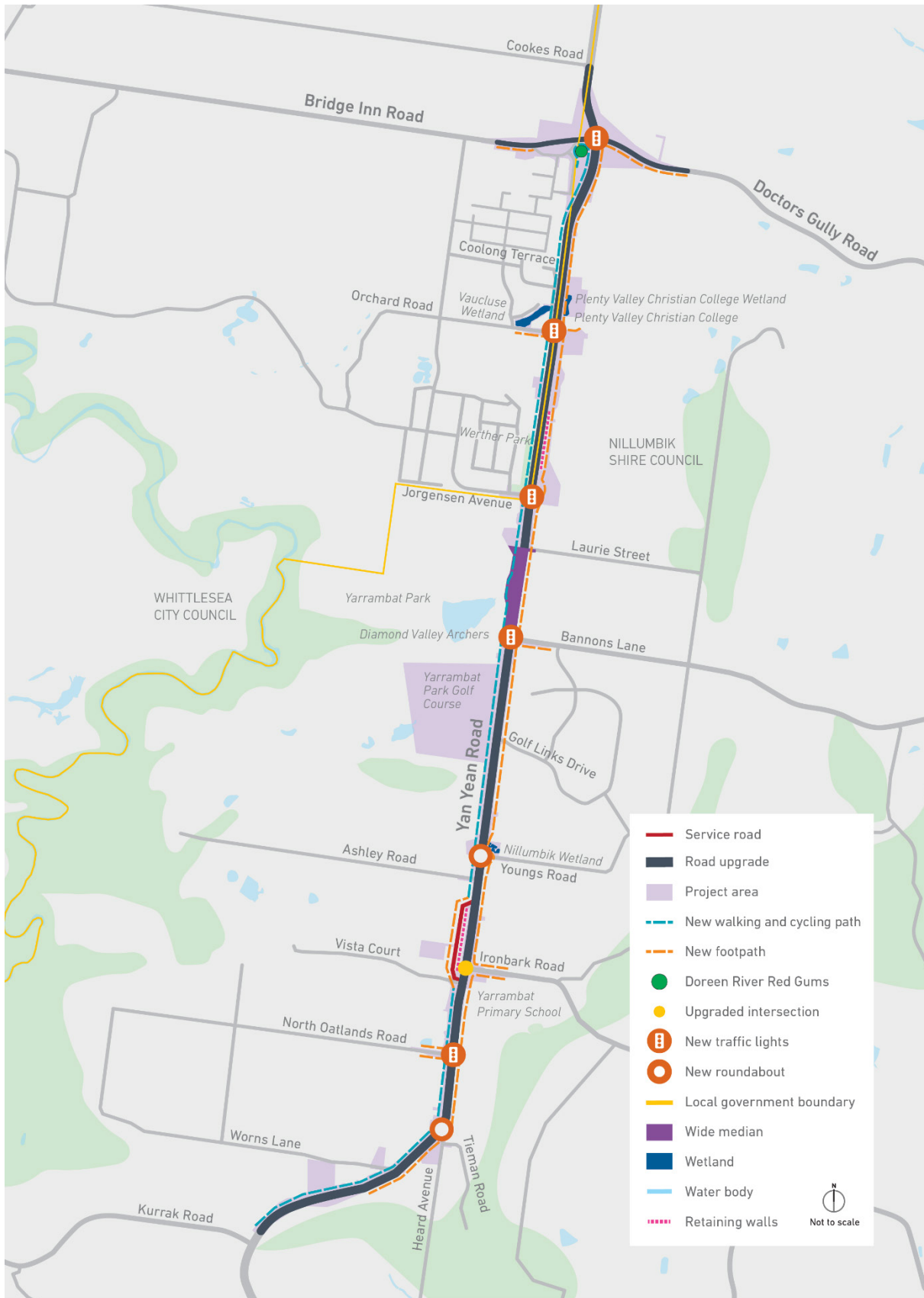
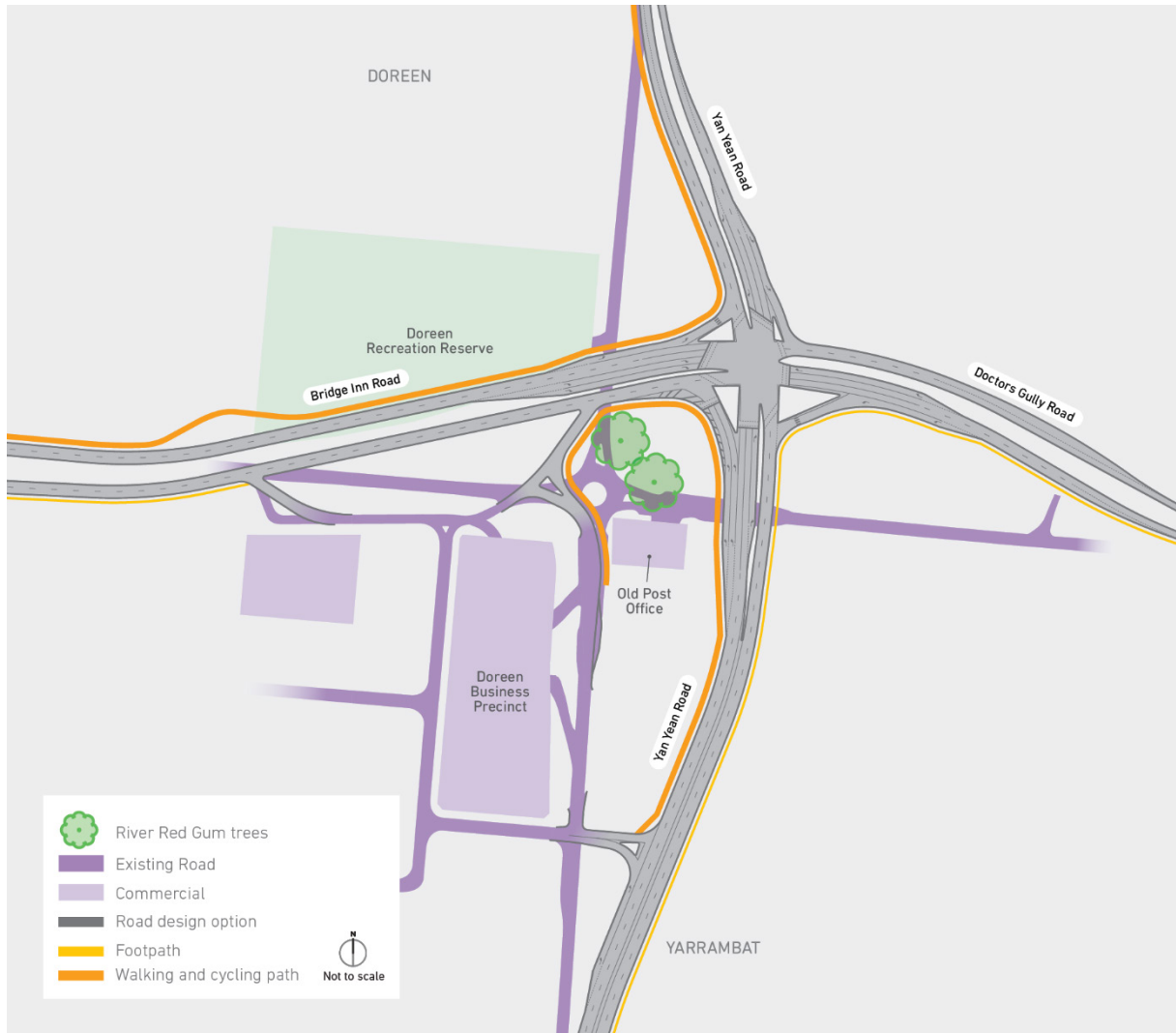


Figure 1.1 Project area

1.4.1 YAN YEAN / BRIDGE INN / DOCTORS GULLY ROAD INTERSECTION

The Yan Yean/Bridge Inn/Doctors Gully Road intersection has been designed to retain the two Doreen River Red Gums, General Store and Pet Supply/Stockfeed business situated adjacent to the current Doctors Gully and Yan Yean Road intersection by shifting the whole intersection to the north east (see Figure 1.2). This intersection design has been developed following community consultation and in response to arboricultural advice on the Doreen River Red Gums.



For illustrative purposes only and subject to change

Figure 1.2 Bridge Inn Road intersection design

1.4.2 CONSTRUCTION ACTIVITIES

Proposed construction activities would likely be standard road construction activities to be undertaken in accordance with the Environmental Performance Requirements (EPRs) for the Project. These construction activities would include:

- tree clearance and vegetation lopping and removal
- establishment of construction site compounds
- clearing and grubbing, temporary sediment and erosion control works
- establishment of environmental and traffic controls
- earthworks, including:
 - remediation of any existing contamination and removal of any hazardous material
 - protecting and relocating services
 - widening of existing rock cuttings (approximately 750 metres of existing cut along the Project would be widened by approximately 20 metres)
 - new cuttings (approximately 1,300 metres of new rock cut would be required to a width of approximately 5 metres along the Project)
 - bulk earthworks and haulage
- civil and structure works, including:
 - roundabouts and intersection upgrades
 - shared user path and pedestrian path construction and connections
 - retaining walls
 - drainage works
 - pavement works
- 30–36 metre high fence along the edge of the Yarrambat Park Golf Course to avoid golf ball collisions with pedestrians, cyclists or vehicles
- traffic management systems and landscaping.

Reference to the project area throughout this document includes all project components listed above.

1.5 PROJECT OBJECTIVES

The Project aims to improve travel times and reliability from growing residential areas in Doreen and Mernda, enhance north-south travel in the area, and improve safety along the corridor. The objectives of the Project are set out below:

- **To improve road safety:** The Project will achieve this by isolating road users from hazards and improving access control through signalised intersections. Congestion and the complex road environment (poor sight lines due to undulating linear/perpendicular grades and adjacent terrain) are presently contributing to the poor safety record on Yan Yean Road.
- **To improve customer experience:** The Project will achieve this by improving access, improving network connectivity, opportunities for active transport, and providing more road capacity.
- **To improve network efficiency:** The Project will achieve improved traffic flow and a reduction in travel times by increasing road capacity and reducing congestion.
- **To maintain environmental and amenity values:** The Project will achieve this by managing environmental effects to acceptable levels and ensuring that impacts are avoided, minimised and mitigated to the extent practicable.

1.6 DESIGN CONSIDERATIONS

Land use planning and environmental risks are a key consideration in the development of the reference design and include:

- reduction in the overall design footprint to minimise the impact on native vegetation and areas of high biodiversity value
- alteration of the road alignment in consideration of the need to avoid and minimise impacts on the topography, built form, land use and acquisition
- reinstatement of driveways and access to all properties and provision of U-turn facilities where access has been restricted including improved safety and accessibility to properties on steep land near the Ironbark Road intersection
- improve safety and connectivity for pedestrians and cyclists with new SUP (west) and footpath (east) linking Diamond Creek to Doreen with future connections to Bridge Inn Road, Mernda Railway Station, Yarrambat Park and Plenty Gorge Parkland Reserve (PGPR)
- facilitate opportunities for links to existing and proposed trail networks including the proposed PGPR Trail which will link MTC with Yarrambat Park.

2 SCOPING REQUIREMENTS

The *Scoping Requirements for Yan Yean Road (Stage 2) Upgrade Environment Effects Statement* (June 2019) have been prepared by the Department of Environment Land Water and Planning (DELWP) on behalf of the Minister for Planning. The Scoping Requirements set out the specific environmental matters to be investigated and documented in the EES, which informs the scope of the EES technical studies.

No specific Draft Evaluation Objectives or Key Issues have been identified in the Scoping Requirements for planning and land use. Despite this, the planning and land use impacts of the Project have been assessed to ensure that adequate studies are undertaken to support the assessment of environmental effects and to demonstrate how the Project will achieve a balance of economic, social and environmental outcomes that contribute to ecologically sustainable development and provide a net community benefit over the short and long-term.

This is consistent with the requirement in the Scoping Requirements to identify the potential adverse environmental effects of the Project and canvass an environmental management approach and performance measures to ensure any effects are identified and avoided, minimised or mitigated.

3 METHODOLOGY

This section describes the method that was used to assess the impacts of the Project on land use. Using the existing conditions as a baseline, a risk based approach was applied to prioritise the key issues to be considered for the impact assessment and inform measures to avoid and minimise potential effects. The following section outlines the methodology adopted for this planning and land use impact assessment.

3.1 STUDY AREA

The study area for the purpose of this planning and land use assessment consists of all land located within the project boundary plus land within a 200 metre buffer directly abutting this boundary. This buffer is considered appropriate to enable key land uses within the vicinity of the Project to be captured and sets the parameters for understanding the existing conditions in the immediate surrounds. However, broader consideration has been given to significant land uses, known significant redevelopments and PSAs outside the study area which have the potential to influence the Project.

3.2 EXISTING CONDITIONS ASSESSMENT

The existing conditions assessment established a baseline of current land use and planning conditions within and adjacent to the project boundary. To establish the existing conditions a detailed desktop assessment was undertaken. This included a review of:

- publicly accessible aerial imagery and street view to identify current land use and built form.
- the legislative framework which applies to the land within the study area including:
 - Commonwealth and State legislation to identify relevant primary approvals
 - key transport legislation, guidelines and policies
- land use planning databases, zoning, overlay mapping and publicly accessible State and local strategic planning documentation. Review of the current and proposed PSAs and planning permits, the Metropolitan Planning Strategy *Plan Melbourne 2017-2050* (Plan Melbourne) and other Growth Corridor Plans to understand the historical intent to upgrade Yan Yean Road and the planned future growth and land use change for the area
- the Whittlesea Planning Scheme (WPS) and Nillumbik Planning Scheme (NPS) including Planning Policy Framework (PPF), Local Planning Policy Framework (LPPF), zones, overlays and relevant particular and general provisions
- review of the land availability plans to identify land which may be required permanently or temporarily for the delivery of the Project
- review of Certificates of Title (CoT) to identify any encumbrances such as covenants, caveats, easements or section 173 agreements (under the *Planning and Environment Act 1987* (P&E Act) that may impact on the delivery of the Project or impact on the established land use character or built form.

The findings of the desktop review are provided in Section 4 and the existing conditions are detailed in Section 5 of this report.

3.2.1 CONSULTATION

MRPV carried out consultation with relevant stakeholders and State and local government agencies and the feedback received was used to inform this assessment. Feedback was provided on existing and likely land use change, land acquisition, access and the identification of Project construction and operation impacts and mitigation measures.

3.3 RISK ASSESSMENT

An Environmental Risk Assessment (ERA) has been completed to identify environmental risks associated with construction and operation of the Project. The risk-based approach is shown in Figure 3.1 and is integral to the EES as required by Sections 3.1 and 4 of the Scoping Requirements and the *Ministerial guidelines for assessment of the environmental effects under the Environment Effects Act 1978*. The land use and planning risk register is provided at Appendix A and the key risks are presented in Section 6.

Primary environmental risks were identified for planning and land use and initial risk ratings were assessed by considering likelihood and consequence criteria (Table 3.2) and applying the risk significance matrix (Table 3.1). The initial risk ratings were assessed assuming the implementation of standard controls. Standard controls include compliance with legislative requirements and best practice requirements typically incorporated into the construction contracts for the delivery of road projects. The standard controls do not include any project-specific controls or requirements.

EPRs have been informed by the ERA, to set the minimum outcomes necessary to avoid, mitigate or manage environmental impacts and reduce environmental risks during delivery of the Project. The development of the proposed EPRs was an iterative process with input from the technical specialists and MRPV. Section 8 provides further detail of the specific EPRs developed for planning and land use.

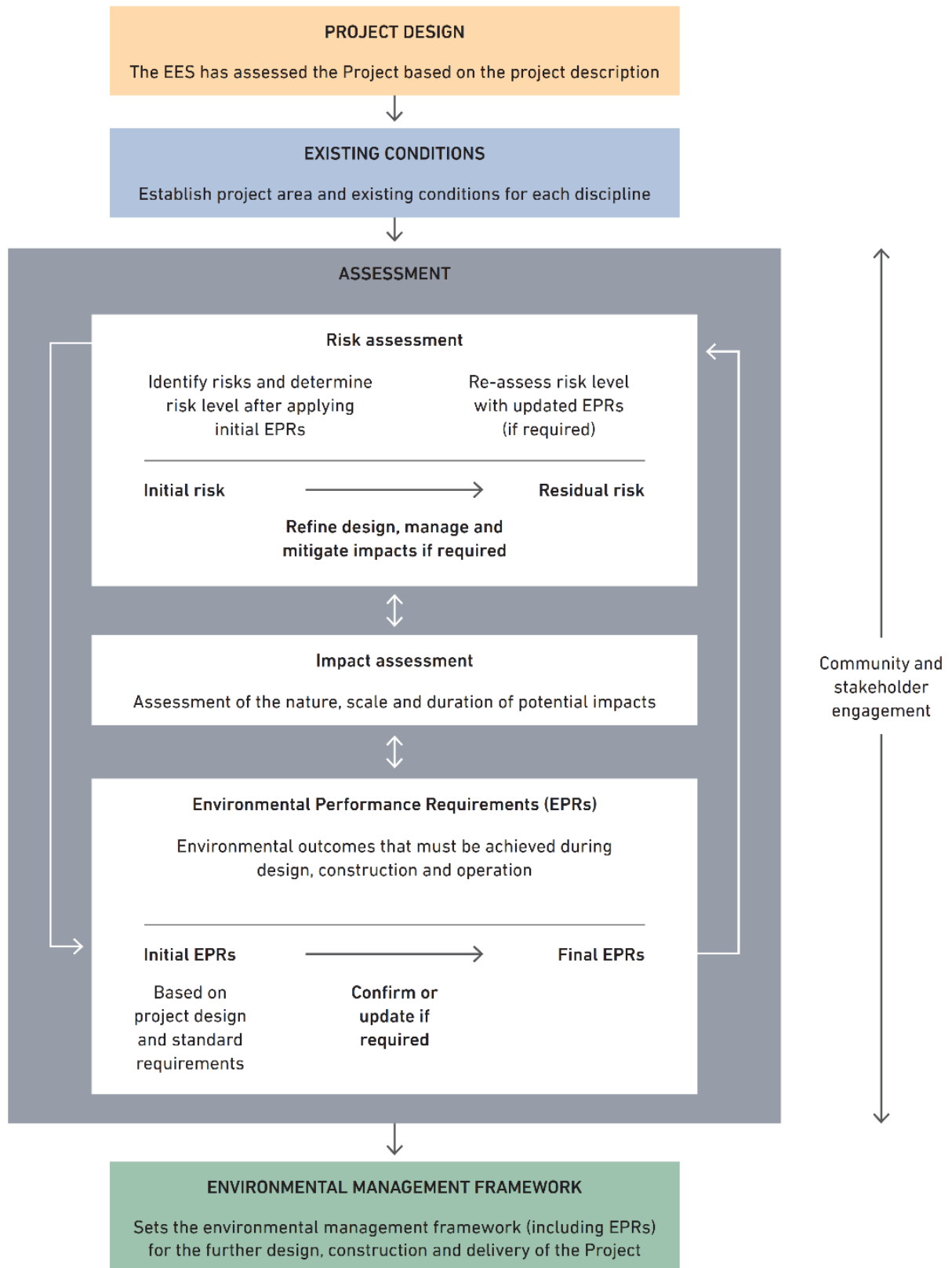


Figure 3.1 Risk assessment process

3.3.1 RISK ASSESSMENT PROCESS

The ERA has guided the environmental studies for the Project. The objectives of the ERA are to:

- identify primary environmental risks that relate to the construction and operation of the Project
- guide the level and extent of investigation and data gathering necessary for accurately characterising the existing environment and assessing the Project's environmental effects
- help identify performance requirements to avoid, minimise and mitigate environmental risks
- inform assessment of likely residual effects that are expected to be experienced after standard controls and proposed EPRs have been implemented.

The risk assessment process for the EES incorporates risk management requirements as detailed in MRPV's Environmental Risk Management Guideline. The process includes:

- an approach to environmental management which is aligned with *ISO 31000 Risk Management – Guidelines*
- systems used to manage environmental risk and protect the environment, and how these are implemented at different stages of road construction, operation and maintenance
- tools and reporting requirements which provide guidance in managing environmental issues throughout the Project.

The ERA identifies impact events for each relevant element of the environment, details the primary risks and has informed the level and range of technical reporting required to address predicted impacts. The ERA utilises a risk matrix approach where likelihood and consequence of an event occurring are considered (Table 3.1, Table 3.2, Table 3.3 and Table 3.4). Throughout the preparation of the EES, the likelihood and consequence criteria were updated to ensure currency, as required.

Table 3.1 Risk significance matrix

LIKELIHOOD	CONSEQUENCE LEVEL				
	Insignificant	Minor	Moderate	Major	Critical
Almost Certain	Medium	Significant	High	High	High
Likely	Medium	Medium	Significant	High	High
Possible	Low	Medium	Medium	Significant	High
Unlikely	Low	Low	Medium	Medium	Significant
Rare	Low	Low	Low	Medium	Medium

Table 3.2 Likelihood criteria

LIKELIHOOD	DESCRIPTION
Almost certain	<p>76-99% Has occurred before and is expected to occur again.</p> <p>Is expected to occur each year or more frequently.</p> <p>All of the controls associated with the risk are extremely weak/non-existent.</p> <p>Without control improvement there is almost no doubt that the risk will eventuate.</p>
Likely	<p>51-75% Has occurred before with a chance of it occurring again.</p> <p>Has occurred several times at the Department, Group, Division, Program or Project before.</p> <p>The majority of the controls associated with the risk are weak.</p> <p>Without control improvement it is more likely than not that the risk will eventuate.</p>
Possible	<p>26-50% Has occurred before with a chance of occurring again.</p> <p>Has occurred at the Department, Group, Division, Program or Project once before.</p> <p>There are some controls that need improvement, however unless there is improvement the risk may eventuate.</p>
Unlikely	<p>6-25% Has occurred elsewhere before, therefore a small chance of occurring.</p> <p>The majority of controls are strong with no control gaps.</p> <p>The strength of this control environment means that is likely that the risk eventuating would be caused by external factors not known to the organisation.</p>
Rare	<p>0-5% Has never occurred but may occur.</p> <p>Is expected to occur 1/100 or more years.</p> <p>All controls are strong with no control gaps.</p> <p>The strength of this control environment means that if this risk eventuated, it is most likely as a result of external circumstances outside of the control of the organisation.</p>

Table 3.3 Generic consequence criteria

CONSEQUENCE	DESCRIPTION
Critical	A critical degree of impact on an environmental asset, value or use of moderate or higher significance.
Major	A high degree of impact on an environmental asset, value or use of moderate or higher significance.
Moderate	A moderate degree of impact on an environmental asset, value or use of moderate or higher significance.
Minor	A low degree of impact on an environmental asset, value or use.
Insignificant	A very low degree of impact on an environmental asset, value or use.

Table 3.4 Planning and land use consequence criteria

ASPECT	INSIGNIFICANT	MINOR	MODERATE	MAJOR	CRITICAL
Land use planning	No fragmentation of land uses or land holdings.	Some minor fragmentation and/acquisition of land uses but properties still able to be used for existing purposes.	Fragmentation/acquisition of land results in 1–10 properties no longer being viable/accessible/useable for existing purpose.	Fragmentation/acquisition of land results in 10–20 properties no longer being viable/accessible/useable for existing purpose.	Fragmentation/acquisition of land results in 20+ properties no longer being viable/accessible/useable for existing purpose.

3.4 IMPACT ASSESSMENT

Land use impacts occur when a project has an effect on the form, function, amenity or appearance of the existing urban environment and/or the character of a place or location. The impact assessment has considered the planning and land use impacts and the future potential land use impacts the Project will or may have on the surrounding land uses. To determine the impact of the Project on land use, the following have been assessed:

- review of the reference design for the Project within the project boundary
- identifying potential land use, planning and economic related impacts during the Project’s construction and operation including:
 - consistency with existing or future land use policy and planning framework (such as PPF, LPPF, zones, overlays and strategic plans)
 - permanent and temporary change in land use due to acquisition (partial or full land acquisition) including impact on business operations
 - permanent and temporary changes to the ongoing use of land from changes in access or amenity
- identification of measures to avoid or minimise potential impacts on land use.

The impact assessment has focused on the potential magnitude, extent and duration of adverse effects as a result of development and operation and is presented in Section 7.

3.4.1 CONSULTATION

As part of this impact assessment, the targeted engagement with key stakeholders undertaken by MRPV has been used to inform proposed land use impacts.

4 LEGISLATION, POLICIES AND GUIDELINES

This section assesses the Project against the relevant Commonwealth and State legislation, policies and guidelines to confirm primary approvals and to identify key planning objectives and strategies that need to be considered in the impact assessment. It provides a planning assessment of the Project against the WPS and NPS and identifies planning permit triggers.

4.1 PRIMARY APPROVALS

The relevant primary legislation that applies to the Project as well as the implications for the Project and any required approvals are summarised in Table 4.1.

Table 4.1 Relevant primary approvals

LEGISLATION	PURPOSE	IMPLICATIONS FOR THE PROJECT
<i>Environment Protection and Biodiversity Conservation Act 1999 (Cwlth)</i> (EPBC Act)	Under the EPBC Act an action (i.e. project, or activity) will require approval from the Minister for the Environment if the action will have or is likely to have a significant impact on Matters of National Environmental Significance (MNES).	The Minister determined that the Project is a 'controlled action' and requires assessment and approval under the EPBC Act. Through the Bilateral Assessment Agreement between the Commonwealth and the State of Victoria this EES will also provide the accredited assessment process to inform the Commonwealth's consideration. Approval Required
<i>Environment Effects Act 1978</i> (EE Act)	The EE Act provides for assessment of the project (works) that may have a significant effect on the environment. Assessment under the EE Act will inform the approval decisions, but will not result in an approval in its own right.	The Minister for Planning issued a decision determining that an Environment Effects Statement (EES) is required for the Project due to the potential for significant environmental effects. This report comprises Technical Appendix L of the Project EES.
<i>Planning and Environment Act 1987</i> (P&E Act)	The P&E Act provides for a framework within which decisions about the use and development of land can be made. The Act sets out the structure and administration of planning in Victoria and authorises the preparation, approval and adoption of planning schemes and PSAs by planning authorities. This act is administered by the Minister for Planning.	The Project is subject to the requirements of the NPS and the WPS and planning approval is required under the P&E Act. A PSA is the preferred approval pathway for this Project. A draft PSA to facilitate the use and development of the Project via an Incorporated Document is proposed. The PSA will also apply Clause 45.01 Public Acquisition Overlay. PSA Required

LEGISLATION	PURPOSE	IMPLICATIONS FOR THE PROJECT
<i>Aboriginal Heritage Act 2006</i> (AH Act)	The AH Act provides that a Cultural Heritage Management Plan (CHMP) must be prepared in circumstances involving both: activities to be completed on land within a legislated area of Aboriginal cultural heritage sensitivity, as described in the <i>Aboriginal Heritage Regulations 2018</i> (Part 2, Division 3); and activities considered to be high impact under the <i>Aboriginal Heritage Regulations 2018</i> (Part 2, Division 5).	Section 52(3) of the AH Act provides that no statutory authorisation can be given before a CHMP is approved. A mandatory CHMP (15169) has been prepared for the Project and will need to be approved prior to construction. CHMP required
<i>Major Transport Projects Facilitation Act 2009</i> (MTPFA)	The MTPFA facilitates the development of major transport projects and aims to streamline the assessment, approvals and delivery of major transport projects in Victoria. Planning and delivery under section 10(1)(a), enables the Premier to declare the entire project a 'declared' project. The planning and delivery regimes in the MTPF Act framework will be available to the Project. For delivery only, the Premier may declare the Project a 'declared' project under section 10(1)(b) but excluding the operations of Parts 3 and 8 of the MTPA. This means that only the delivery regimes in the MTPFA framework will be available to the Project.	The use of the MTPFA for delivery provides greater powers to the delivery agency than would be available under other legislation i.e. <i>Land Acquisition and Compensation Act 1986</i> , <i>Road Management Act 2004</i> , and <i>Crown Land (Reserves) Act 1978</i> . MRPV may seek legislative powers under section 10(1)(b) of the MTPFA for delivery, and that the Project be declared (except for parts 3 and 8) under the Act. This can be done following the approval of the PSA. The Project may be declared under section 10(1)(b) of the MTPF Act for delivery purposes.
<i>Land Acquisition and Compensation Act 1986</i> (LA&C Act)	The LA&C Act outlines the process for the acquisition of land by State Government for public purposes, either compulsorily or by negotiation. The Act also provides procedures for the determination of compensation.	A total of 96 parcels require partial or full acquisition for the Project. Of these parcels, 60 parcels are private. Section 5 of the Act states that an authority cannot commence to acquire the land " <i>unless the land has been first reserved by or under a planning instrument for a public purpose</i> ". If the Project is declared under section 10(1)(b) of the MTPFA, there is a requirement to comply with the provisions of the LA&C Act, as modified by the MTPFA, to the extent that any interest in land needs to be compulsorily acquired for the Project. A Public Acquisition Overlay (PAO) is proposed to be applied over land required for acquisition via the PSA.

LEGISLATION	PURPOSE	IMPLICATIONS FOR THE PROJECT
<p><i>Crown Land (Reserves) Act 1978</i> (CL Reserves Act)</p>	<p>The CL Reserves Act provides for the reservation of land for certain public purposes (including roads and parks) by the Governor in Council and sets out the administrative and legal framework for managing reserved Crown land and the processes for revoking Crown land reservations. Approval is required from the Minister for Energy, Environment and Climate Change or the relevant Committee of Management to use and occupy crown land.</p>	<p>Land to the south of Jorgensen Avenue, Doreen (807 Yan Yean Road) is reserved for the PGPR which is managed by Parks Victoria. It contains the Yarrambat Picnic Area. The balance of the recreation land fronting the west of Yan Yean Road is Yarrambat Park which is owned and managed by NSC as district open space.</p> <p>Approval from the Minister for Energy, Environment and Climate Change is required.</p> <p>If the Project is declared under section 10(1)(b) of the MTPFA for delivery then the land will be subject to orders under the MTPFA reserving the land for the purposes of this Project.</p>
<p><i>Environment Protection Act 1970</i> (EP Act)</p>	<p>The Project must consider impacts to the environment as outlined by the relevant legislation and supporting regulations and guidelines. In particular State Environment Protection Policies (SEPPs) are policies made under the provisions of the EP Act to provide detailed requirements and guidance in relation to environmental matters. There are a number of different SEPPs which deal with issues such as air quality, noise, water, waste management and gas emissions. SEPPs apply to all businesses, non-government agencies, community groups, individuals, and State and local government agencies, to the extent they are relevant.</p> <p>Of relevance to this Project are:</p> <ul style="list-style-type: none"> — SEPP (Prevention and Management of Contamination of Land) No. S95 — SEPP (Waters) No. S499 – commenced on 19 October 2018 — <i>Construction Techniques for Sediment Pollution Control, 1991</i> — <i>EPA Victoria: Doing it right on subdivisions: Temporary environmental protection measures for subdivision construction sites (EPA 2004)</i> — <i>Environmental Guidelines for Major Construction Sites, 1996.</i> 	<p>The Project must consider impacts to the environment as outlined by the relevant SEPPs.</p>

LEGISLATION	PURPOSE	IMPLICATIONS FOR THE PROJECT
<i>Environment Protection Amendment Act 2018</i>	The <i>Environment Protection Amendment Act 2018</i> is in preparation and will provide the foundation for the transformation of Victoria's environment protection laws and the EPA. This Act focuses on preventing waste and pollution impacts rather than managing those impacts after they have occurred. New guidelines are under development by EPA and will be released following implementation of the <i>Environment Protection Amendment Act 2018</i> .	<p>Due to the COVID-19 outbreak and necessary response by the Victorian Government, commencement of the <i>Environment Protection Amendment Act 2018</i> has been postponed until 1 July 2021.</p> <p>As the proposed construction and operation dates of the Project will be post July 1 2021, construction and operation activities must have regard to the new Act.</p>

4.1.1 SECONDARY CONSENTS

Secondary consents are identified within the other technical reports outlined in Section 1. The Project may trigger the following:

- Works on Waterways permit under the *Water Act 1989*
- a permit to take protected flora under the *Flora and Fauna Guarantee Act 1988*
- a permit under the *Wildlife Act 1975*, if wildlife is to be handled or relocated from the construction area
- a permit under the *Catchment and Land Protection Act 1994*, if noxious weeds or material containing noxious weeds or weed seed is transported from the Project construction area.

4.1.2 TRANSPORT LEGISLATION, POLICY AND GUIDELINES

A summary of relevant transport legislation, policy and guidelines for the project is provided in Table 4.2.

Table 4.2 Transport legislation, policy and guidelines

LEGISLATION, POLICY AND GUIDELINES	PURPOSE	IMPLICATIONS FOR THE PROJECT
<i>Transport Integration Act 2010</i> (TI Act)	<p>The TI Act is the principal transport statute for Victoria and establishes a common policy framework for use by state and local government bodies when making decisions about the transport system. It achieves this through the vision statement, transport system objectives and decision-making principles set out in the Act. The vision for transport in Victoria is:</p> <p><i>“an integrated and sustainable transport system that contributes to an inclusive, prosperous and environmentally responsible state.”</i></p>	<p>The Project must be consistent with all relevant requirements of the TI Act having regard to the transport system objectives and principles identified in this legislation. This requirement is triggered for projects likely to have a significant impact on the transport system.</p> <p>An assessment of the Project against the objectives and principles of the TI Act is provided within the Explanatory Report which forms part of the PSA for the Project.</p>

LEGISLATION, POLICY AND GUIDELINES	PURPOSE	IMPLICATIONS FOR THE PROJECT
<i>Towards Zero 2016-2020, Victoria's Road Safety Strategy & Action Plan</i>	Provides an integrated strategy that involves government, communities, vehicle manufacturers, transport companies with an implementation budget of \$1 billion and has a goal of reducing road deaths to 200 or below by 2020. It also aims to reduce serious injuries by 15% over this period.	The Project will upgrade, improve and increase the capacity of the road. The provision of a separated footpath and SUP as well as safety barriers will have a positive impact on safety in line with the objectives of Towards Zero.
<i>Northern Horizons 2016</i>	Regional Development Australia (RDA) is a partnership between the Australian, state and territory and local governments to support the growth and development of Australia's regions. <i>Northern Horizons 2016</i> was developed as a collaboration between RDA and eight northern councils. It assesses current infrastructure provision in the region and sets out targets for future development to support economic and population growth.	The strategy highlights the upgrade of Yan Yean Road as a medium priority project necessary to improve existing connections north of the M80 Ring Road and east of the Hume Highway.

4.2 PLANNING SCHEMES

The P&E Act is the primary legislative framework for the regulation of land use and development and provides a framework within which land use planning decisions can be made. It provides the framework for planning schemes, which contain State and Local Government policy together with the zone, overlay, particular and general provisions that apply to each municipal area in Victoria. The Project is affected by the provisions of the NPS and the WPS (the Schemes) which control the use and development of land within the municipality. Relevant sections of the Schemes are identified below.

4.2.1 PLANNING POLICY FRAMEWORK

The PPF sets the overall context for spatial planning and decision making for Victoria. It seeks to ensure that the “objectives of planning in Victoria are fostered through appropriate land use and development planning policies and practices”. The PPF must be taken into account by planning authorities when preparing PSAs. Key statewide and regional planning policies that apply to the use and development of the land for the Project are outlined in Appendix C and include:

- Clause 11 Settlement
- Clause 12 Environmental and Landscape Values
- Clause 13 Environmental Risks and Amenity
- Clause 14 Natural Resource Management
- Clause 15 Built Environment and Heritage
- Clause 18 Transport
- Clause 19 Infrastructure.

Of particular relevance to the Project is Clause 18 Transport which states that “Planning should ensure an integrated and sustainable transport system that provides access to social and economic opportunities, facilitates economic prosperity, contributes to environmental sustainability, coordinates reliable movements of people and goods, and is safe”.

Clause 18 seeks to integrate land use and transport planning (18.01-1S Land Use and Transport Planning), the transport system (18.01-2S Transport system), sustainable personal transport within inner and outer Melbourne (18.02-1S Sustainable Personal Transport, 18.02-1R Sustainable Personal Transport - Metropolitan Melbourne), principal public transport network (18.02-2S Public Transport, 18.02-2R Principal Public Transport Network), ensuring an adequate supply of car parking that is appropriately designed and located (Clause 18.02-4S Car Parking), and developing the road system to include freight links in inner and outer Melbourne (18.02-3S Road System, 18.05-1S Freight Links, 18.05-1R Freight Links – Metropolitan Melbourne).

4.2.2 LOCAL PLANNING POLICY FRAMEWORK

In line with the transitional provisions of Amendment VC148, the LPPF of the planning scheme provides the local context and planning policies specific to the WPS and NPS, until the future introduction of the Municipal Planning Strategy and integration of local content into the PPF. Therefore, consideration has been given to the Municipal Strategic Statement (MSS) and local planning policies (LPPs) which together form the LPPF. The MSS provides the context, vision and strategic framework and the strategic justification for application of zones and overlays. The local planning policies guide decision making in relation to a specific discretion in a zone, overlay or particular provision. It aids decision making by providing greater understanding of how a project will be considered and what will influence decision making.

The key aspects of the MSS and LPPs for each municipal area are outlined in Appendix D. These clauses provide an assessment of the strategic land use framework and the objectives and strategies which guide decision making. The relevant EES technical reports provide detail on policies relevant to other considerations of the Project.

4.3 PLANNING ASSESSMENT

The primary use of the Project is for a road. A road is defined under section 3 of the P&E Act and includes “*a highway, street, lane, footway, square, court, alley or right of way, whether a thoroughfare or not and whether accessible to the public generally or not*”. The Project would also result in the relocation of minor utility installations from within the road reserve or within land being acquired to facilitate the construction of the Project. Minor Utility Installation is defined as land used for a utility installation comprising any of the following: (a) sewerage or water mains; (b) storm or flood water drains or retarding basins; (c) flow measurement device or a structure to gauge waterway flow; (d) gas mains providing gas directly to consumers; (e) a sewerage treatment plant, and any associated disposal works, required to serve a neighbourhood; (f) a pumping station required to serve a neighbourhood; (g) power lines designed to operate at less than 220,000 volts but excluding any power lines directly associated with an energy generation facility or Geothermal energy extraction; or (h) an electrical sub-station designed to operate at no more than 66,000 volts but excluding any sub-station directly associated with an Energy generation facility or Geothermal energy extraction.

The Project has been assessed against the relevant zones, overlays, particular and general provisions within each of the schemes to identify the relevant planning permit triggers and requirements. This assessment is summarised below and a detailed assessment is provided within Appendix D.

4.3.1 GENERAL EXEMPTIONS

There are general exemptions from permit requirements for roads under Clause 62 of the NPS and WPS. Under Clause 62.01 the requirement for a permit for the use of land, does not apply to:

“The use of land for a Road except within the Urban Floodway Zone and a Public Conservation and Resource Zone.

The use of land for a minor utility installation”.

In addition, under Clause 62.02-1 building and works not requiring a permit include “*buildings and works associated with a minor utility installation*” under Clause 62.02-2, unless it is specifically required by the planning scheme. Any requirement in the scheme:

“relating to the construction of a building or the construction or carrying out of works, other than a requirement in the PCRZ, does not apply to roadworks.

Under Clause 62.04 Subdivisions not requiring a permit states that any requirement in this scheme relating to the subdivision of land does not apply to:

A subdivision by an authority acquiring the land which does not create an additional lot.

Under Clause 62.05 Demolitions does not require a permit for:

“the demolition or removal of a building or works unless a permit is specifically required for demolition or removal”

Unless specifically mentioned, the exemptions under Clause 62.01, Clause 62.02-2, Clause 62.04 and 62.05 apply to the zones and overlays described in the tables below.

4.3.2 ZONES AND OVERLAYS

The Project has been assessed against the zones and overlays within the project boundary and a summary of key planning permit triggers is provided in Table 4.3. The zoning and overlay maps are provided at Appendix D.

Table 4.3 Summary of planning permit triggers – zones and overlays

PLANNING PROVISION	PLANNING REQUIREMENTS
City of Whittlesea	
Clause 42.02 – Vegetation Protection Overlay (VPO1) Schedule 1 – Significant Vegetation (River Redgum Grassy Woodland)	A planning permit is required.
Nillumbik Shire	
Clause 35.06 – Rural Conservation Zone (RCZ3) Schedule 3 – Conservation Values	A planning permit is required for earthworks which change the rate of flow or the discharge point of water across a property boundary.
Clause 42.01 – Environmental Significant Overlay (ESO1) Schedule 1 – Sites of Faunal and Habitat Significance	A planning permit is required to remove vegetation not specifically listed under Clause 42.01-3. The ecology assessment will confirm the extent of vegetation removal under this overlay.
Clause 43.01 – Heritage Overlay (HO191) ‘River Red Gums (2)’	A planning permit is required.
Clause 43.01 – Heritage Overlay (HO219) ‘St Michael’s Anglican Church’	A planning permit is required.

4.3.3 PARTICULAR PROVISIONS

Particular provisions of the Schemes also apply to the Project in addition to the zone and overlay controls as described above. The particular provisions relevant to the Project are detailed in Table 4.4 below.

Table 4.4 Particular provisions relevant to the Project

PARTICULAR PROVISIONS	PURPOSE	PERMIT REQUIREMENT
Clause 51.02 – Metropolitan Green Wedge Land: Core Planning Provisions	To protect metropolitan green wedge land from uses and development that would diminish its agricultural, environmental, cultural heritage, conservation, landscape natural resource or recreation values.	Subdivision of land to create a lot that is smaller in area than the minimum area specified for the land in the zone is prohibited under Clause 51.02. However, in instances where land will be acquired and subdivided from existing lots within the Green Wedge Zone and Rural Conservation Zone, the subdivision is exempt under Clause 62.04. The requirements of this provision also do not apply to the Special Use Zone 3, Plenty Valley Christian College, Yan Yean Road, Doreen. A planning permit is not required.
Clause 52.02 – Easements, Restrictions and Reserves	To enable the removal and variation of an easement or restrictions to enable a use or development.	A planning permit will be required to create, vary or remove an easement or restriction.
Clause 52.17 – Native Vegetation	To ensure permitted clearing of native vegetation results in no net loss in the contribution made by native vegetation to Victoria's biodiversity.	A planning permit is required for the removal of native vegetation.
Clause 52.29 – Land adjacent to a Road Zone, Category 1 or a Public Acquisition Overlay for a Category 1 Road	To ensure appropriate access to existing and future arterial roads, as well as the appropriate subdivision of land adjacent to identified roads. Planning permit applications must be referred to the owner/acquiring authority.	A planning permit is required to create or alter access to a road in a Road Zone Category 1, and/or land in a PAO.
Clause 52.33 – Post Boxes and Dry Stone Walls	To conserve historic post boxes and dry stone walls.	A planning permit will be required to demolish or remove a post box constructed before 1930. A planning permit will be required to demolish, remove or alter a dry-stone wall constructed before 1940.
Clause 53.02 – Bushfire Planning	To ensure that the development of land prioritises the protection of human life and strengthens community resilience to bushfire.	A planning permit is not required under this provision.

4.3.4 PLANNING APPROVAL PATHWAY

The proposed planning pathway for the use and development associated with the Project is via a PSA to the NPS and WPS. The Amendment would amend the NPS and WPS to apply Clause 45.12 Specific Controls Overlay and Clause 45.01 Public Acquisition Overlay to affected land, and insert the *Yan Yean Road (Kurrak Road to Bridge Inn Road) (Stage 2) Upgrade Project Incorporated Document* into the Schedules to Clause 45.12 and Clause 72.04. This would facilitate the use and development of the land for the Project subject to the specific controls outlined in the Incorporated Document. In accordance with the *Ministerial Guidelines for Assessment of Environmental Effects under the EE Act (Ministerial Guidelines) 2006*, the draft Amendment GC92 would be exhibited concurrently with the EES and a joint Inquiry and Advisory Committee would be appointed to consider submissions on the draft PSA and EES. The Minister for Planning's assessment of the EES would inform the consideration of the draft PSA and the Commonwealth Minister for the Environment's approval decisions under the EPBC Act.

5 EXISTING CONDITIONS

This section of the report provides a description of the existing land use within and adjacent to the project boundary to provide the local context of the road and frames the Project within its broader regional context, where relevant. A review of the existing strategic plans which guide the use and development of land adjacent to the road corridor has also been undertaken.

5.1 LOCAL CONTEXT

Yan Yean Road is approximately 12.7 kilometres long extending from Diamond Creek Road in the south (Plenty), to Arthurs Creek Road in the north (Doreen). It provides a significant north-south connection that links the communities of Plenty, Yarrambat and Doreen. It also provides connectivity to east-west arterials including Bridge Inn Road, Kurrak Road and Diamond Creek Road linking residents to the suburbs of Mernda, South Morang, Diamond Creek and Greensborough (Figure 5.1).

The section of Yan Yean Road between Kurrak Road and Diamond Creek Road is a designated VicRoads arterial and works to upgrade this section comprise the Yan Yean Road Upgrade – Stage 1. These works were completed in late 2019. This project, Yan Yean Road Upgrade Stage 2 comprises the section of Yan Yean Road between Kurrak Road and Bridge Inn Road which is also an arterial road while the section north of the Bridge Inn Road intersection will remain a Council road and no works are currently proposed.

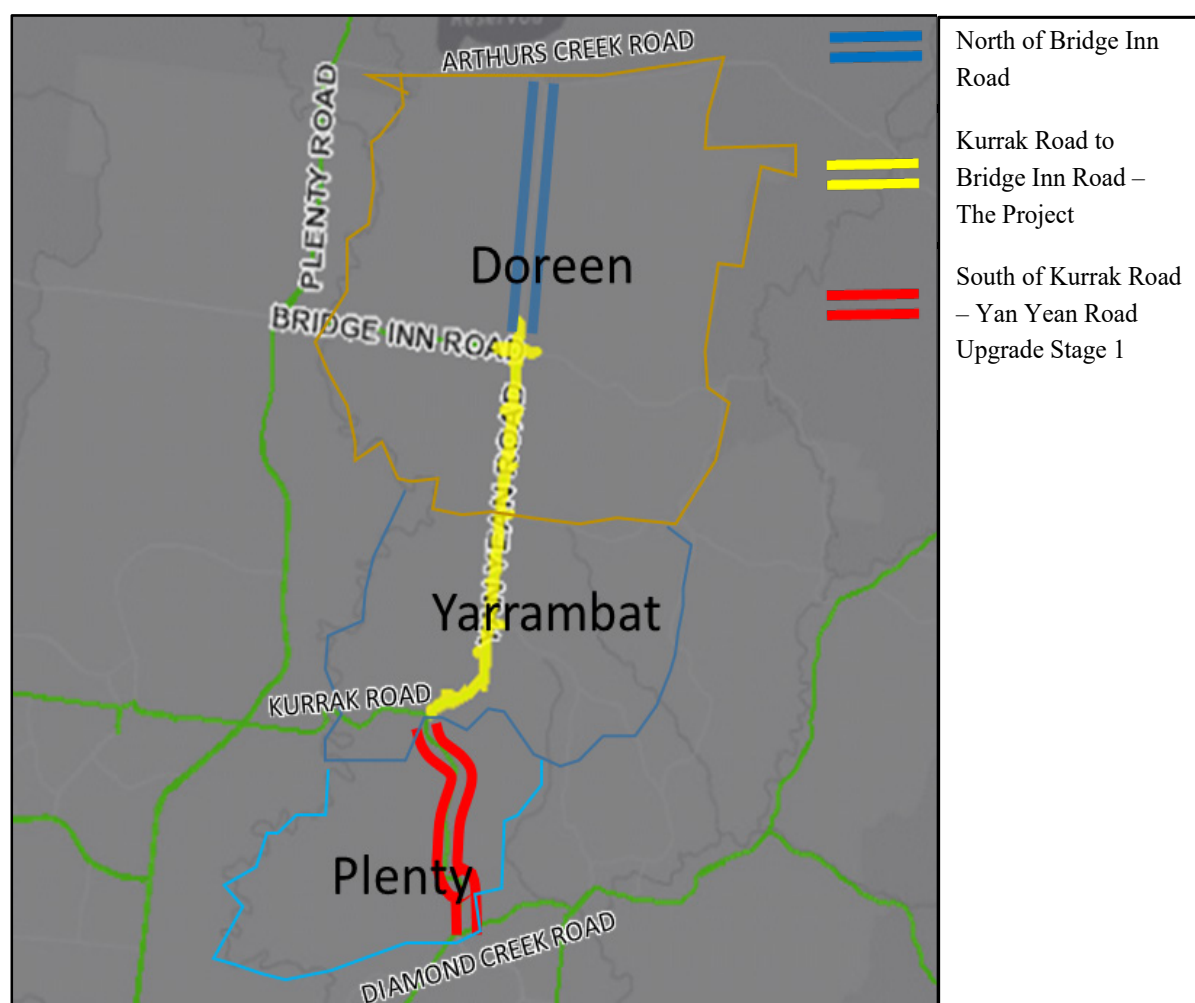


Figure 5.1 Yan Yean Road context map

5.2 TIMELY PROVISION OF TRANSPORT INFRASTRUCTURE

Melbourne is experiencing rapid population growth, a changing economic geography and an expanding freight task, particularly in the western, northern and south-eastern outer suburbs. The City of Whittlesea is one of the five fastest growing municipalities within Metropolitan Melbourne and the suburbs of Doreen, South Morang and Mernda have experienced a combined growth of nearly 50,000 people or 300% between 2006 to 2016 (Australian Bureau of Statistics, Regional Population Growth 2016). This growth correlates with an increase in traffic counts which show a consistent upward trend between 2006 and 2017 of more than 150% (just south of Bridge Inn Road).

The *Victoria in Future 2019* (VIF) projections from 2018 to 2036, indicate the City of Whittlesea needs to accommodate an additional 141,130 residents (223,320 to 364,450). Population projections for smaller areas prepared on behalf of WCC and NSC (forecast.id) indicate that Doreen, Plenty and Yarrambat areas are to continue to grow with more than 10,000 residents expected to move into these areas between 2016–2031. An additional 19,000 residents are expected to reside in the Mernda and South Morang areas in the same timeframe. At the southern end of Yan Yean Road, the Plenty-Wattle Glen region is expected to grow by an additional 3,400 people by 2031 (VIF 2016).

The suburb of Doreen alone, has experienced the second fastest rate of population growth in Australia. It has expanded from a population of 2,733 people in 2006 to 21,904 people in 2016 which represents a growth of 700% over 10 years (The Australian Bureau of Statistics Regional Population Growth Summary 2016). Traditionally, transport infrastructure investment in the outer suburbs has not kept pace with the rate of growth and pattern of settlement and the limited capacity on the arterial road network is leading to increased congestion, reduced reliability, higher travel costs and has safety implications through higher accidents and incidents. Residents within the Nillumbik green wedge have a high level of car dependence due to lack of public transport and the need to travel outside of the municipality for work and services. More than 75% of people commute to other locations for work and has the highest rate of daily travel to work places outside the Shire of all the interface councils (NSC 2011). This high level of car dependence within the green wedge contributes to increased congestion, travel time and maintenance costs.

5.3 EXISTING TRAFFIC CONDITIONS

The rapid population growth highlighted above is contributing to poor travel conditions being experienced along Yan Yean Road. The road is now at, or exceeding, capacity and journey times are increasing and becoming more unreliable. This impacts private and public travel times and safety.

Yan Yean Road carries between 20,000 and 24,000 vehicles on an average weekday, depending on the section. To the north of Ironbark Road, as many as 25,000 vehicles are observed on a typical Thursday or Friday. Yan Yean Road is at or close to capacity during the morning and evening peak periods with key constraints at the intersections with Ironbark Road (morning) and Bridge Inn Road (evening). Average journey speeds in the morning peak (southbound) are approximately 35 km/h along the entire length and can be as low as 25 km/h at times. Traffic queues generated at the Ironbark Road intersection are occasionally extend as far as 2 kilometres (to Jorgensen Avenue). Average journey speeds for the return (northbound) commuter trip in the evening peak are similar, with traffic queues generated at the Bridge Inn Road occasionally spill back to Orchard Road (and beyond).

Road safety on this section of Yan Yean Road is of critical importance. In a 5-year period between 2014–2018, there were a total of 38 crashes recorded resulting in one fatal and nine serious injury crashes. The fatal head on collision occurred just north of Jorgensen Avenue in 2014. The intersection at Jorgensen Avenue qualified for upgrade funding under the Commonwealth Government’s Black Spot Program in 2015–2016 which seeks to target road locations where crashes are occurring and implement funding measures to reduce the national road toll.

In response to the urgent need and the timely provision of transport infrastructure, the State Government has committed to a number of current and proposed projects for the North Growth Corridor including; Plenty Road Upgrade (McKimmies Road to Bridge Inn Road); Yan Yean Road Stage 1; Northern Roads Upgrade Project (including Childs, Epping, Craigieburn and Bridge Inn Roads Upgrade) and the Mernda Rail Extension Project (MREP). These projects, combined with the duplication of the Project from two lanes to four lanes, will add significant capacity and improve regional connectivity. The Project will also improve the regional arterial road network to metropolitan Melbourne via the Metropolitan Ring Road (M80) and further connections to the Hume Freeway (M31) and the future North East Link.

5.4 EXISTING ROAD

Yan Yean Road is a 12 kilometre section of roadway, classified as an M3 road for “*Moderate movement of people and/or goods within a municipality*” in accordance with DoT’s *Movement and Place in Victoria Framework 2019*. It currently functions as a key route between Diamond Creek Road and Bridge Inn Road and a local road between Bridge Inn Road and Arthurs Creek Road. The Project extent was classified as a council road until around 2015, when VicRoads took ownership and was declared as a secondary arterial road.

It is currently an undivided two lane road, consisting of sections with very steep grade and poor sight lines. The cross section is rural in nature with unsealed shoulders, adjacent open table drains and existing trees close to the road’s edge. Direct access to abutting land use is generally uncontrolled and a majority of intersections priority controlled via ‘give way’ or ‘stop’ control. Bridge Inn Road and Orchard Road are controlled by roundabouts and there are traffic signals at Ironbark Road. The speed limit on the road is currently 70 km/h with two 40 km/h school zones at Orchard Road and Ironbark Road. A tree reserve exists on the eastern side of Yan Yean Road to restrict access to properties from Yan Yean Road (GTA 2013).

Yan Yean Road is currently serviced by two public bus routes including the 381 (Mernda Station to Diamond Creek Station) and 385 (Greensborough to Mernda North). During the morning and evening peaks there is a combined frequency of 4 to 5 buses per hour (per direction). The DoT *Movement and Place in Victoria Framework 2019* classification at the intersection with Bridge Inn Road is a P4 – Place of Neighbourhood importance due to the local off-street convenience centre. Between Bridge Inn Road and Jorgensen Avenue, Yan Yean Road is part of the Principal Bicycle Network (PBN) and provides connectivity north to Arthurs Creek and Yan Yean Reservoir and connects to the east-west running PBN along Bridge Inn Road with access to Mernda Station. There are currently no formal facilities for cycling on-road and the narrow width of the road is likely to currently discourage its use by cyclists. Furthermore, footpaths are provided along 25% of Yan Yean Road, generally only on one side. There are short lengths of footpaths generally found in the vicinity of Plenty Valley Christian College (Orchard Road to Bridge Inn Road) and Yarrambat Primary School (around Ironbark Road) typically to provide access to local bus stops and local residential areas.

5.5 EXISTING LAND USE

Much of the Nillumbik Shire is used for agriculture, rural living and conservation purposes and the residential population is concentrated into larger centres to the east and south-east of the Yarrambat/Plenty Corridor. The Project is located in a predominantly low density residential and rural living area within the metropolitan Green Wedge. The northern western end of the Project is within the Whittlesea Growth Corridor which is experiencing rapid land use change from rural living to residential. A land use map (Figure 5.2) and detailed description of the land uses is provided below.

5.5.1 RESIDENTIAL

Land to west of Yan Yean Road from Cookes Road to Jorgensen Avenue is covered by the *Mernda Local Structure Plan – Part 1 1994* and the MSP which established the use and development framework for Doreen and Mernda. From Cookes Road to Jorgensen Avenue, large rural lots are being subdivided into standard residential lots ranging in size from approximately 400 sqm and 1000 sqm in area with a frontage of 20 metres. With the resultant land use change there are a number of areas along the alignment which are yet to be built out, including the residential subdivision at Clarke Avenue (under construction) and land which is still in planning including the three land holdings at 815–835 Yan Yean Road, south of Orchard Road. The latter is subject to *Development Plan 815–835 Yan Yean Road, 2017* which earmarks the site for residential lots of 800 sqm at the interface with Yan Yean Road with standard and medium density lots extending west. Allowance has been made within the Development Plan for the widening of Yan Yean Road at the intersection with Orchard Road however, it has not accounted for road widening on the site's eastern extent. These residential areas are coloured blue on Figure 5.2.

5.5.2 OPEN SPACE

The land between Cookes Road and Bridge Inn Road to the west of Yan Yean Road forms part of Precinct 2A within the MSP. Plan 1.1 Precinct Boundaries of the MSP identifies that the land affected by the Project is located within an existing transmission line easement (Ausnet) and at the present time remains largely undeveloped.

To support the future residential growth, land on the north-western corner of the Bridge Inn Road intersection is being developed by WCC as the Doreen Recreation Reserve. The Doreen Recreation Reserve currently comprises 1.2 hectares and its facilities include a cricket ground, toilets, tennis courts and car parking.

To the west of Yan Yean Road, south of Jorgensen Avenue, the predominate land use is recreation and open space. Portion of this land is located within the PGPR, managed by Parks Victoria, and the balance of this land is within Yarrambat Park, which is managed by NSC. The section of the PGPR which fronts Yan Yean Road contains a picnic area and car park which are well setback off the road behind established vegetation. Yarrambat Park currently includes a range of active recreation facilities, including an 18-hole golf course, driving range and a number of sporting and recreation clubs including archery, fly fishing, horse riding, historical society and model aircraft. This land is coloured green on Figure 5.2.

5.5.3 COMMERCIAL

South of Bridge Inn Road and on the western side of Yan Yean Road is a partially developed commercial centre accessed via Yellow Brick Road. This includes a number of food outlets, mechanical garage, service station and a childcare centre. In addition, south-east of the Bridge Inn Road and Yan Yean Road intersection, on the opposite side of the road, is local businesses including a General Store with an Australia Post outlet and associated dwelling; and a pet and stock supply retail store located at 920/920A Yan Yean Road.

Concentrated around and accessed via Ashley Road, on the west side of Yan Yean Road, are a number of commercial uses. These commercial uses differ from rural enterprise due to their scale and the size of the lots. These include kennels, horse paddock, veterinary services, an egg farm and a childcare centre.

To the east of Yan Yean Road and accessed via Ironbark Road, is the service centre for the Yarrambat township which contains the Yarrambat General Store, St Michael's Church, Tancks Corner Café and a Community Hall. To the south-west of the Project alignment, is the Yan Yean Stage 1 Project Office and beyond the residential areas at the south of the project boundary, is the Nillumbik Recycling and Recovery Centre. This land is coloured purple on Figure 5.2.

5.5.4 RURAL LIVING / CROPPING

To the east, beyond the interface with Yan Yean Road, land is being used for broad-acre cropping and grazing. These lots are approximately 8 hectares containing limited built form which is setback from the boundary. This use has an interface with Yan Yean Road north of Laurie Street and lots at this interface range from 2 to 12 hectares. This land is coloured red on Figure 5.2.

5.5.5 RURAL ENTERPRISE

Land to the east of Yan Yean Road, to the north and south of the Bridge Inn Road intersection, is used for rural enterprises. These include racehorse breeding and agistment situated on 63 hectares with a frontage of 400 metres to Yan Yean Road. In addition, to the east of Yan Yean Road well setback beyond rural residential uses and north of Laurie Street, is the Yarrambat Estate Vineyard. Rural enterprise land is coloured brown on Figure 5.2.

5.5.6 EDUCATION

Plenty Valley Christian College is located opposite Orchard Road to the east of Yan Yean Road. The college site is subject to Clause 51.02 of the NPS and planning for this site has been informed by the *Plenty Valley Christian College Master Plan, 2008*. A cluster of educational uses are located at Ironbark Road to the east of Yan Yean Road. These include Yarrambat Primary School, Yarrambat Plenty Preschool and Glenvale School Melbourne Campus. The sites are large in size with extensive built form and sporting facilities which are well setback from the road. These facilities are accessed from Ironbark Road and De Fredericks Road whilst Yarrambat Primary School also has a secondary access from Yan Yean Road. This land is coloured yellow on Figure 5.2.

5.5.7 RURAL RESIDENTIAL

To the east of Yan Yean Road land use is characterised by rural-residential allotments with the exception of a ribbon of smaller residential lots between Laurie Street and Bannons Lane, which are generally 1200 sqm with a frontage of 22 m. In general, the rural residential lots are 8 hectares in area with a frontage of 180 metres and built form is set back from the road and accessed via long driveways.

The alignment intersects the township of Yarrambat. The settlement pattern for the area is lots typically between 1 and 3 hectares on the western side of Yan Yean Road and greater than 3 hectares on the eastern side of Yan Yean Road which contain residential land uses. The property at 590 Yan Yean Road contains an unoccupied dwelling and an additional new dwelling which is currently subject to an active planning permit. A condition of the active planning permit requires the unoccupied dwelling to be demolished once the new dwelling is completed.

Planning for the township formalised a landscape reserve along the frontage of sites abutting Yan Yean Road to restrict access, established a 30 metre building setback along the road, to maintain its rural character and preserve the opportunity for any road widening which might be required in the longer term. This land is coloured pink on Figure 5.2.

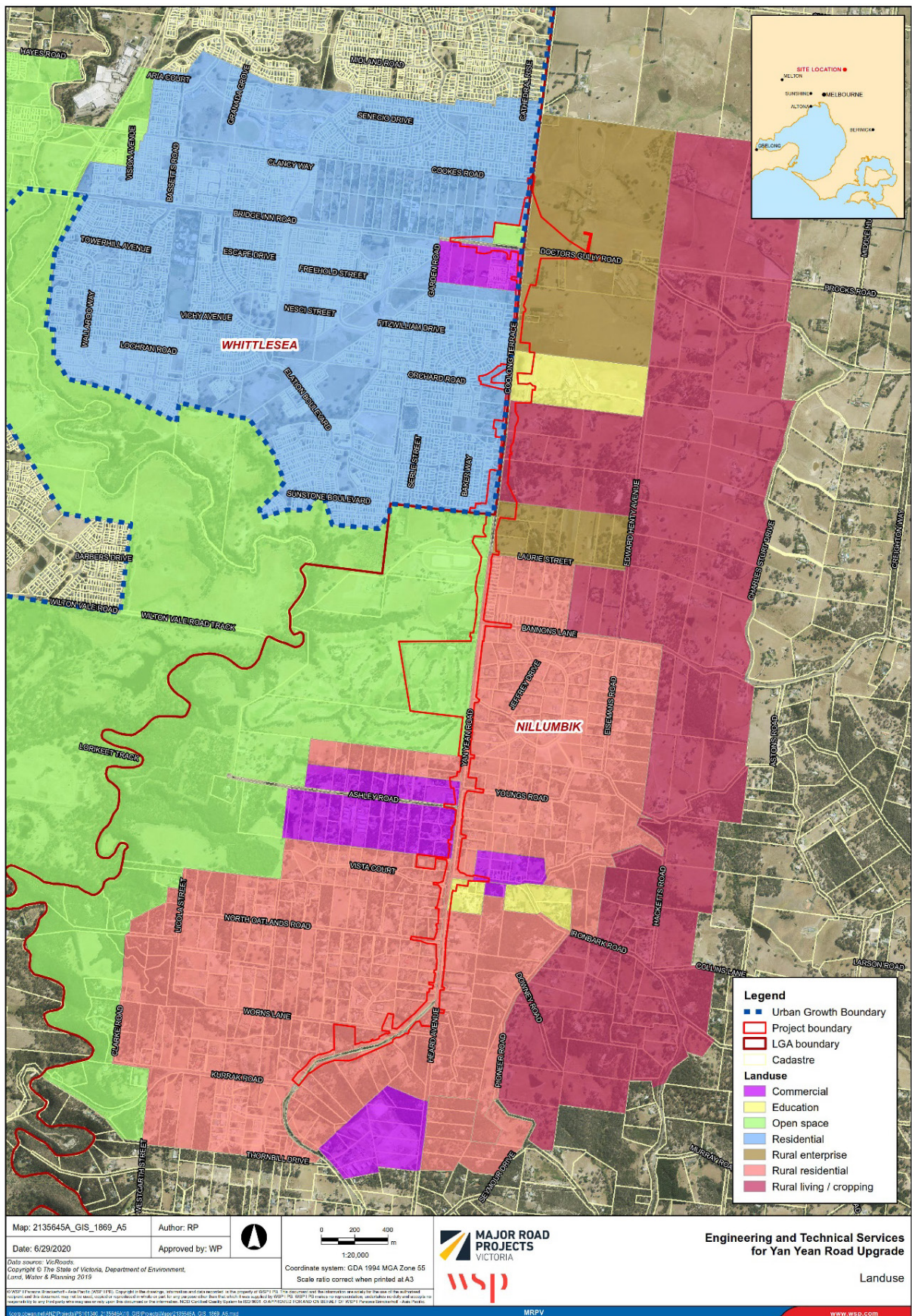


Figure 5.2 Land use map

5.6 STRATEGIC PLANNING CONTEXT

The need to upgrade Yan Yean Road to a secondary arterial road to support the development of the North Growth Corridor was identified as early as 1990 within the *Plenty Valley Strategic Plan*. Subsequent strategic plans provided for a 30 metre building setbacks and restricted access to facilitate its future upgrade. These early decisions shaped the land use and settlement pattern within Doreen, Yarrambat and Plenty.

5.6.1 HISTORICAL CONTEXT

An overview of the history of the growth of the northern corridor in relation to Yan Yean Road is provided below:

- *Planning Policies for Metropolitan Melbourne 1971* delineated the first growth corridors and green wedges that would define the future growth patterns of Melbourne. This included the Plenty Corridor and the associated green wedge.
- In 1987, the State Government's Metropolitan Policy, *Shaping Melbourne's Future* announced that outward growth would generally be channelled into the Plenty Valley in the north, Berwick and Pakenham in the south-east, and Werribee in the west.
- The *Plenty Valley Strategic Plan 1990* is an incorporated document of the WPS and a key document in the historic and strategic basis for urban development at Mernda/Doreen. The Plan included a proposed upgrade to multilane divided carriageway roads for Plenty Road and Yan Yean Road to provide north/south connections and Bridge Inn Road and Findon Road to provide east/west arterial road connections. It also reinforced the role of Nillumbik as an interface municipality which allowed for larger lots along the Plenty River fringe to provide a transition area between the conservation areas of Plenty Gorge and adjacent residential areas to the east (Clause 21.03-2 Rural Land Use of the NPS).
- The subsequent Local Structure Plans (LSP) which were prepared for each area within the Plenty corridor include South Morang, Mernda/Doreen and the Whittlesea Township. The north-east portion of Mernda/Doreen (*Mernda Local Structure Plan – Part 1 (Mernda LSP Part 1)*) was approved in 1994 – Whittlesea Amendment L199). Mernda LSP Part 1 established the framework for Laurimar Park (west of Yan Yean Road) and Mernda Township. Subdivision of Doreen began in 1999 with the establishment of the 334 hectares, 2,500 lot subdivision of Laurimar, west of Yan Yean Road. A town centre with shops, community hall, a primary school (2009) and a linear park were established at Laurimar.
- The *Yarrambat Local Structure Plan 1993* (Revised 2000) and *Yarrambat Extension Area Local Structure Plan 1995* (Revised 2000) were incorporated into the NPS and established the strategic land use and development framework for the coordinated development of rural residential land in Yarrambat. It sought to encourage rural housing on larger lots which would be serviced by the existing facilities in Ironbark Road and acknowledged the role of Yan Yean Road as a secondary north-south arterial road that all other local roads intersect.
- Changes to local government boundaries required the gazettal of the planning schemes for Whittlesea in 1996 and Nillumbik in 1997. More recent changes with the introduction of the VPPs in 1998, new schemes were gazetted for Whittlesea in 1999 and for Nillumbik in 2000.
- The Plenty Valley Strategic Plan was approved by the Victorian State Government in 1990 and initiated amendments to the Whittlesea and then Diamond Valley Planning Schemes to facilitate this process. The plan was the culmination of a rigorous planning exercise that considered the entire Plenty Valley Growth Corridor. This process confirmed the future of Mernda as an urban growth corridor and laid the foundations for the current Mernda Strategy Plan 2004 (Amended 2016). The Plan was incorporated into the Whittlesea Planning Scheme via Amendment C30.

- Released in 2002, *Melbourne 2030* directs growth to activity centres and the five designated growth areas and identifies 12 green wedges. It also introduced the Urban Growth Boundary as a tool to manage the outward growth of metropolitan Melbourne.
- *A Plan for Melbourne's Growth Areas 2005* and the *Precinct Structure Plan Guidelines* set out a strategic approach for development in Melbourne's growth corridors and established the *Whittlesea Growth Area Plan* which included the suburbs of Epping North, South Morang and Mernda. The plan provided enough land for Whittlesea to grow by 40,000–66,500 people and accommodate 46,000 jobs. The Growth Area Plan identified key north-south and east-west links within Whittlesea and provided for a network of existing and new activity centres at South Morang, Mernda and Epping North. The *Whittlesea Growth Area Framework Plan* provided at Appendix F shows the planned urban growth adjacent to Yan Yean Road and reinforces Mernda as a Major Town Centre.
- Growth Areas Authority (GAA) was created in 2006 to address the lack of coordinated planning and development across greenfield areas. The Victorian Government reviewed the urban growth boundary in 2010 after it released *Melbourne @ 5 million*, the update to *Melbourne 2030* (2008). Approximately 6,000 hectares, including land within the Melbourne northern growth corridor (Hume – Mitchell- Whittlesea Growth Area) was included within an expanded urban growth boundary to include 'logical inclusions' by an Independent Advisory Committee.
- Growth Corridor Plans were developed in 2012 by the then GAA (now Victorian Planning Authority (VPA)) to provide an integrated approach to planning land use, transport and services in Melbourne's outer growth areas. The northern section of Yan Yean Road forms the eastern boundary of the North Growth Corridor. A key objective of the *North Growth Corridor Plan* is, "*extending the northern region's public transport and arterial road networks into the Growth Corridor so that future residents and workers will enjoy a similar level of accessibility to those living and working in established parts of the north*".

5.6.2 PLAN MELBOURNE

Plan Melbourne, released in March 2017, sets out the State Government's vision for the City to 2050 (this a 'revision' of the previous Plan Melbourne released in May 2014). One of the key challenges identified in the strategy is "*keeping up with the growing transport needs of the city*", with congestion and overcrowding an issue on the road network, particularly at peak times (DELWP 2017).

One of the key outcomes of Plan Melbourne is that Melbourne is a productive city that attracts investment, supports innovation and creates jobs. Plan Melbourne recognises the importance of creating jobs close to where people live. *Plan Melbourne* identifies key national employment and innovation clusters (NEIC), health and education precincts, significant transport gateways and industrial precincts. In addition, Metropolitan Melbourne has a network of activity centres which are identified within Plan Melbourne and classified into three main types; Metropolitan Activity Centre (MAC), Major Activity Centre and Neighbourhood Activity Centres. Plan Melbourne states "*diversification gives communities access to a wide range of goods and services, provide local employment and support local economies and the development of 20-minute neighbourhoods*". Policy 1.2.1 seeks to "*support the development of a network of activity centres, linked by transport*". Yan Yean Road is part of the arterial road network which provides access to Mernda, Diamond Creek and Greensborough Major Activity Centres and La Trobe NEIC. The Project will improve north-south connectivity and capacity to these important economic areas. The future MTC and integrated public transport hub will serve as a regional hub to 50,000 people in the wider catchment.

A key outcome of Plan Melbourne is that "*Melbourne has an integrated transport system that connects people to jobs and services and goods to market*". The Project will support the rapid growth of Mernda/Doreen and improve arterial road connectivity to the M80, La Trobe NEIC, Melbourne Airport and the Northern Industrial Precinct. Two of the key directions supporting this outcome is to "*transform Melbourne's transport system to support a productive city*" and to "*improve transport in Melbourne's outer suburbs*". These have associated policies relating to improving arterial road connections across Melbourne, specifically Policy 3.2.1 "*improve*

roads in growth areas and outer suburbs”. The Project will contribute to an improved arterial road network, leading to greater efficiencies, improved productivity, and greater access to jobs, services and markets. Policy 3.1.6 focuses on “supporting cycling for commuting” by establishing strategic cycling corridors. The Project will introduce off road cycling facilities linking into the future Bridge Inn Road SUP and south into the Yan Yean Road Stage 1 SUP.

5.6.3 PLENTY GORGE PARK RESERVE MASTERPLAN 2018

The PGPR is a metropolitan park which extends 11 kilometres along the Plenty River from Mernda to Bundoora. PGPR is managed by Parks Victoria and has been reserved as public land under the *Crown Land and Reserves Act 1978*. Parks Victoria have developed the *Plenty Gorge Park Reserve Masterplan 2018* (Masterplan 2018) for PGPR which was approved in October 2018. The master plan provides guidance on improving the park to meet the open space needs of the rapidly growing surrounding community. A key objective of the Masterplan 2018 is to “connect the Mernda station and town centre to Yarrambat and communities to the east”. Recommendation Y01 as shown within Figure 5.3 below is to “construct a shared trail to connect Mayfield site with Yarrambat” and Y02 is to “collaborate with VicRoads and Council to improve safe public and vehicle access into Yarrambat Park, including potential alternative access from Jorgensen Avenue.” Recommendation Y03 is to “work with Nillumbik Council to improve connections with Yarrambat Park facilities”.

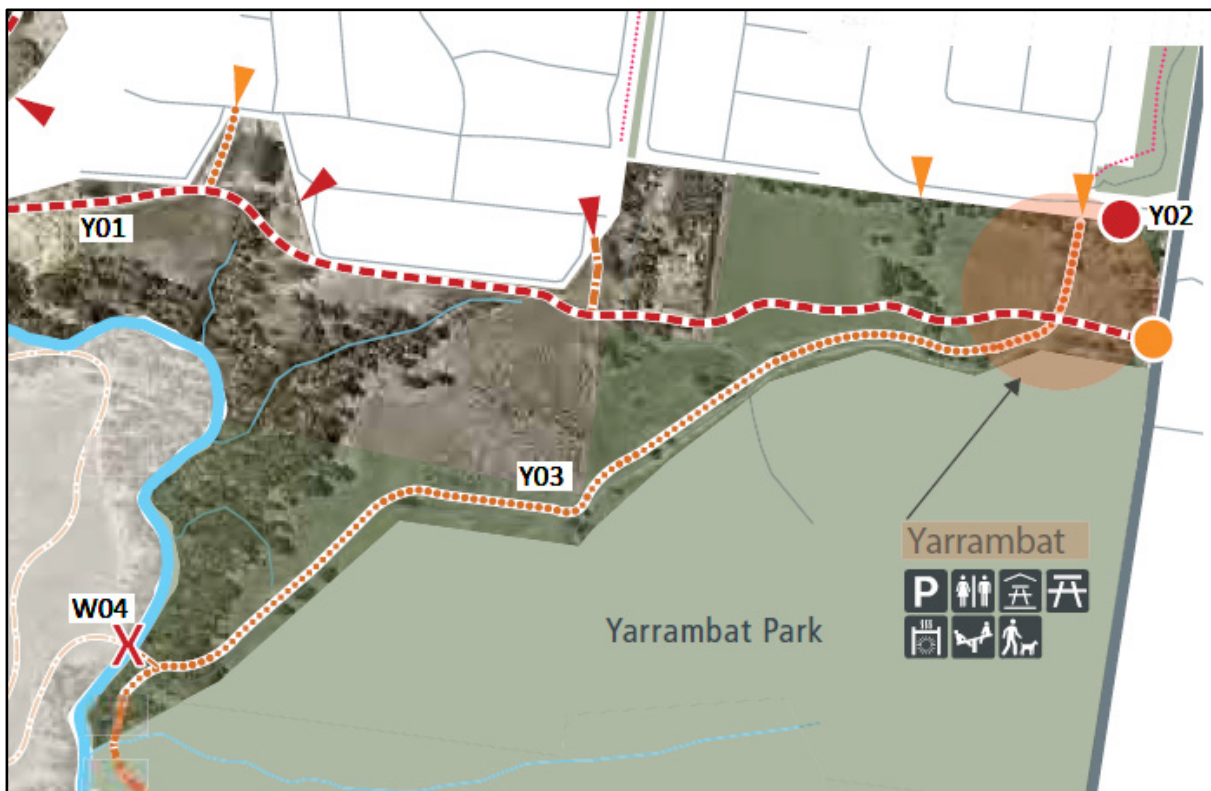


Figure 5.3 Plenty Gorge Park Reserve Masterplan 2018 – Park Area 2 Yarrambat

5.6.4 WHITTLESEA CITY COUNCIL STRATEGIC PLANS

This section sets out the relevant strategic plans for the study area including those prepared or approved by City of Whittlesea.

5.6.4.1 MERNDA STRATEGY PLAN

The *Mernda Strategy Plan, 2004* (Amended via C123 in 2017) (MSP), incorporated into the WPS via Amendment C123, sets out the land use and development framework for the Mernda growth area. The MSP growth area covers approximately 1,700 hectares and is projected to support around 14,000 dwellings and

associated community infrastructure. Over 50 hectares of land has been identified for the MTC supporting retail, commercial and an employment park. The Comprehensive Development Zone (CDZ) has been applied to this land to enable a range of employment-generating land-uses that are compatible with nearby residential and commercial development. Within the MSP (Figure 5.4), Yan Yean Road is identified as an important north-south secondary arterial road to support the growth of Mernda/Doreen and the introduction of high capacity rail to this area through the MREP. The MSP also includes the proposed cross section for Yan Yean Road which is outlined in Figure 5.5.

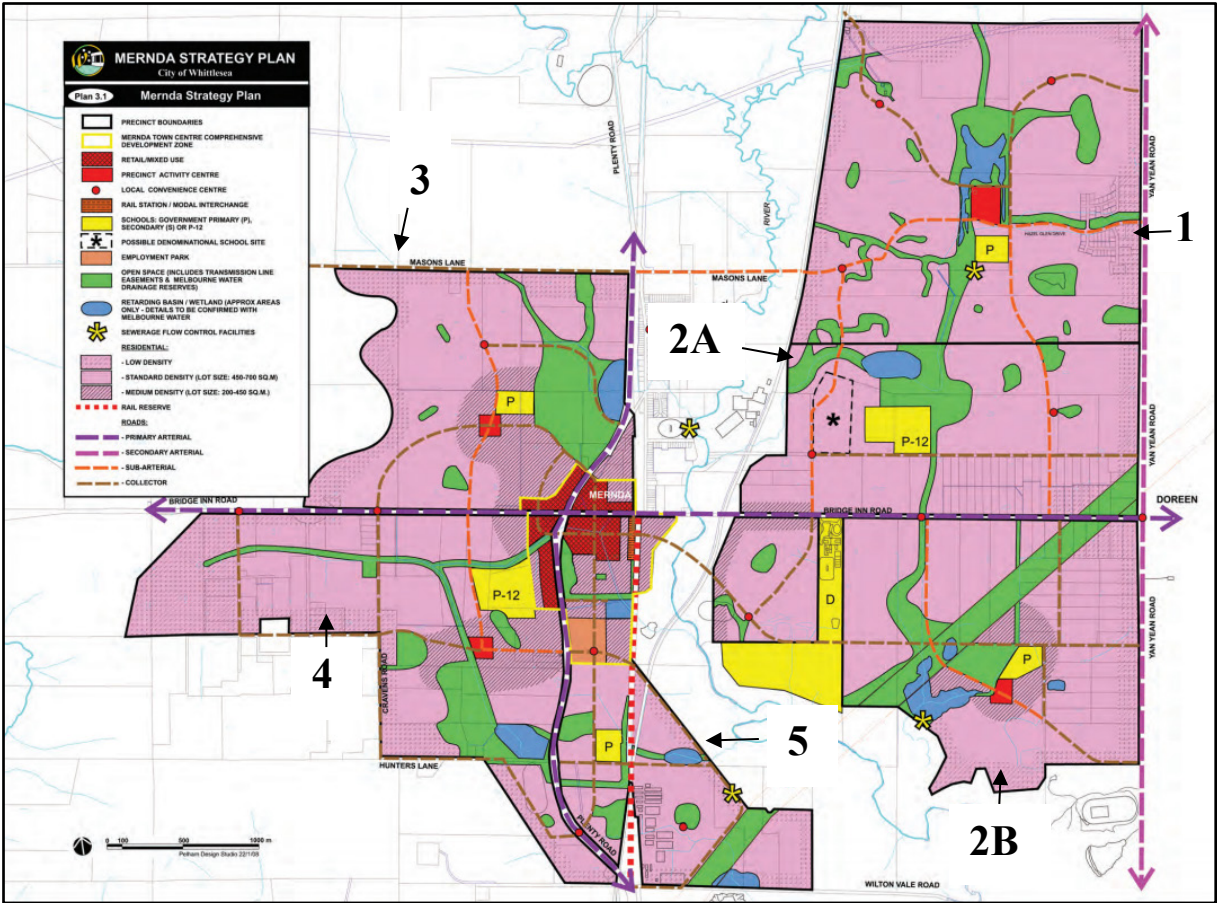


Figure 5.4 Strategic land use framework for Mernda/ Doreen (MSP)

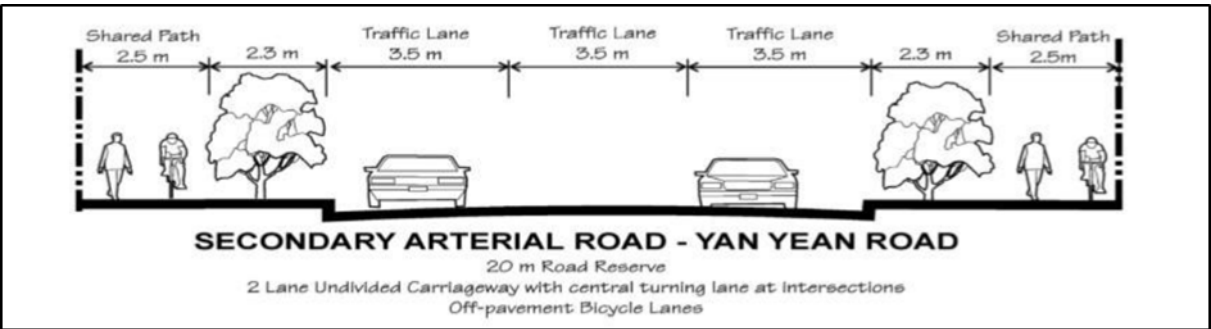


Figure 5.5 Proposed cross section for Yan Yean Road (MSP)

5.6.4.2 DOREEN RESERVE MASTERPLAN

Doreen Recreation Reserve Master Plan 2014 sets out the vision for the redevelopment of the recreation reserve, shown within Precinct 2A at the north-west corner of the Yan Yean Road and Bridge Inn Road intersection (Figure 5.4). The Master Plan (Figure 5.6) has been prepared by WCC with the understanding that future widening and intersection upgrade of Yan Yean Road and Bridge Inn Road would be required in the near future. The document however, has not been incorporated into the WPS nor is it a reference document. The Plan proposes the redevelopment of the site, including the removal of existing assets and creation of new tennis courts, car parking and site access that are inset from the existing road alignments. The Plan also shows potential areas for future expansion of the reserve.



Figure 5.6 Doreen Recreation Reserve Master Plan (Source: City of Whittlesea)

5.6.4.3 DEVELOPMENT PLAN FOR 815–835 YAN YEAN ROAD

The role of the Development Plan for 815–835 Yan Yean Road is to facilitate the integrated development of the three undeveloped lots on the south side of Orchard Road in the context of the principles within the MSP. These include restricted access to Yan Yean Road. The Development Plan will be the basis for future subdivision and development of the land. The Development Plan was approved by WCC on the 18 April 2017 and construction has commenced on the two northern lots. The Development Plan provides buffer vegetation to be retained along Yan Yean Road with larger lots abutting to provide an interface with the rural land opposite. The Development Plan recognises that Yan Yean Road will need to be widened in the future and provides for the widening of the intersection with Orchard Road (area shaded orange in Figure 5.7 below). The Development Plan further states that the “exact extent of the road widening shall be determined in consultation with Council and VicRoads at the time of the planning permit application for subdivision on the abutting land”.



Figure 5.7 815-835 Yan Yean Road Development Plan

5.6.4.4 OTHER STRATEGIC DOCUMENTS

Additional strategic documents prepared by WCC are detailed within Table 5.1.

Table 5.1 WCC strategic documents

CITY OF WHITTLESEA STRATEGIC DOCUMENTS	
<i>City of Whittlesea Integrated Transport Strategy 2014</i>	<p>The <i>Integrated Transport Strategy 2014</i> sets out the principles City of Whittlesea has adopted to address transport related challenges over the coming 20 years. The Strategy identifies key concerns of local residents, with traffic congestions being one of the most dominant, noting that infrastructure has not kept pace with the rapid population growth. Furthermore, the need for improved road network connections is highlighted as a top priority for residents. The strategy also seeks to promote and encourage active transport as a viable alternative to car usage through the provision of appropriate infrastructure.</p> <p>The Project will facilitate a safer and more efficient road network, improving the connection between residents and local services. The Project addresses key community concerns and meets the objectives of the Strategy.</p>
<i>City of Whittlesea Road and Public Transport Plan 2017</i>	<p>The <i>Whittlesea Road and Public Transport Plan 2017</i> is an outline of the issues, actions and possible solutions to moving more people in and around the City of Whittlesea. The Plan addresses the rapid population growth and resulting ‘congestion crisis’ on the roads of Whittlesea. The Plan identifies the duplication of Yan Yean Road from the end of the current duplication project at Kurrak Road to Bridge Inn Road and its urbanisation to Arthurs Creek Road. This will complete the arterial road network and enable Doreen and Mernda residents to have improved access to employment and transport hubs at Greensborough and Eltham. Specifically, the duplication from Kurrak Road to Bridge Inn Road and urbanisation to Arthurs Creek Road is recognised as a very high priority within the Plan.</p>
<i>City of Whittlesea Heritage Study 1990</i>	<p>The City of Whittlesea Heritage Study 1990 recommended the Post Office / General Store be given Planning Scheme Protection. The grading given to the property was D indicating “sites are either reasonably intact representatives of particular periods or styles, or they have been substantially altered but stand in an area which retains much of its original character”. However, no protection was afforded to the buildings. In addition, the two Red Gums were recommended for Planning Scheme Protection and Grade C. The recommendations of the study were to restrict parking on roots and ensure road widening does not threaten trees. In 1994 the municipal council boundaries were reformed and the City of Whittlesea ceded land, including the Post Office/General Store and the two Red Gum trees, to NSC.</p>

5.6.5 NILLUMBIK SHIRE COUNCIL STRATEGIC PLANS

This section sets out the relevant strategic plans for the study area including those prepared or approved by NSC.

5.6.5.1 YARRAMBAT LOCAL STRUCTURE PLAN

The *Yarrambat Local Structure Plan 1993*, revised 2000 and *Yarrambat Extension Area Local Structure Plan 1995* (revised 2000) are incorporated into the NPS and established the strategic land use and development framework for the coordinated development of rural residential land in Yarrambat. The LSP seeks to encourage the redevelopment of the area with detached housing on one hectare lots and the rural residential community will be serviced by the existing facilities in Ironbark Road. In general, residential lots abutting Yan Yean Road at this point comprise one hectare lots whilst those behind, accessed via alternative roads, comprise up to 3 hectares in size. The Project will not impact on the potential for these lots to be subdivided to create one hectare lots in line with the LSP. The Plan, provided at Figure 5.8, reinforces the role of Yan Yean Road as a secondary north-south arterial road, and that all other local roads intersect with and feed into it. It identifies capacity constraints on Yan Yean Road due to increasing through traffic as the road connects the Yarrambat and neighbouring communities *“to regional and metropolitan destinations including the Greensborough District Centre and the Metropolitan Ring Road”*. The plan identifies that *“provision needs to be made for widening of Yan Yean Road, and for the upgrading of intersections along Yan Yean Road”*.

As stated previously the Plan formalised a landscape reserve along the frontage of sites abutting Yan Yean Road to restrict access and applied a 30 metre building setback along the road. This is consistent with the strategies under Clause 21.05-1 Settlement and Housing which *“encourage the provision of tree reserves where the edge of a township or urban area abuts a main or secondary road and maintain existing non-urban breaks between rural townships and between urban and non-urban areas”*. These plans were also supported by development contribution plans (DCP) which made provision for levies for future drainage.

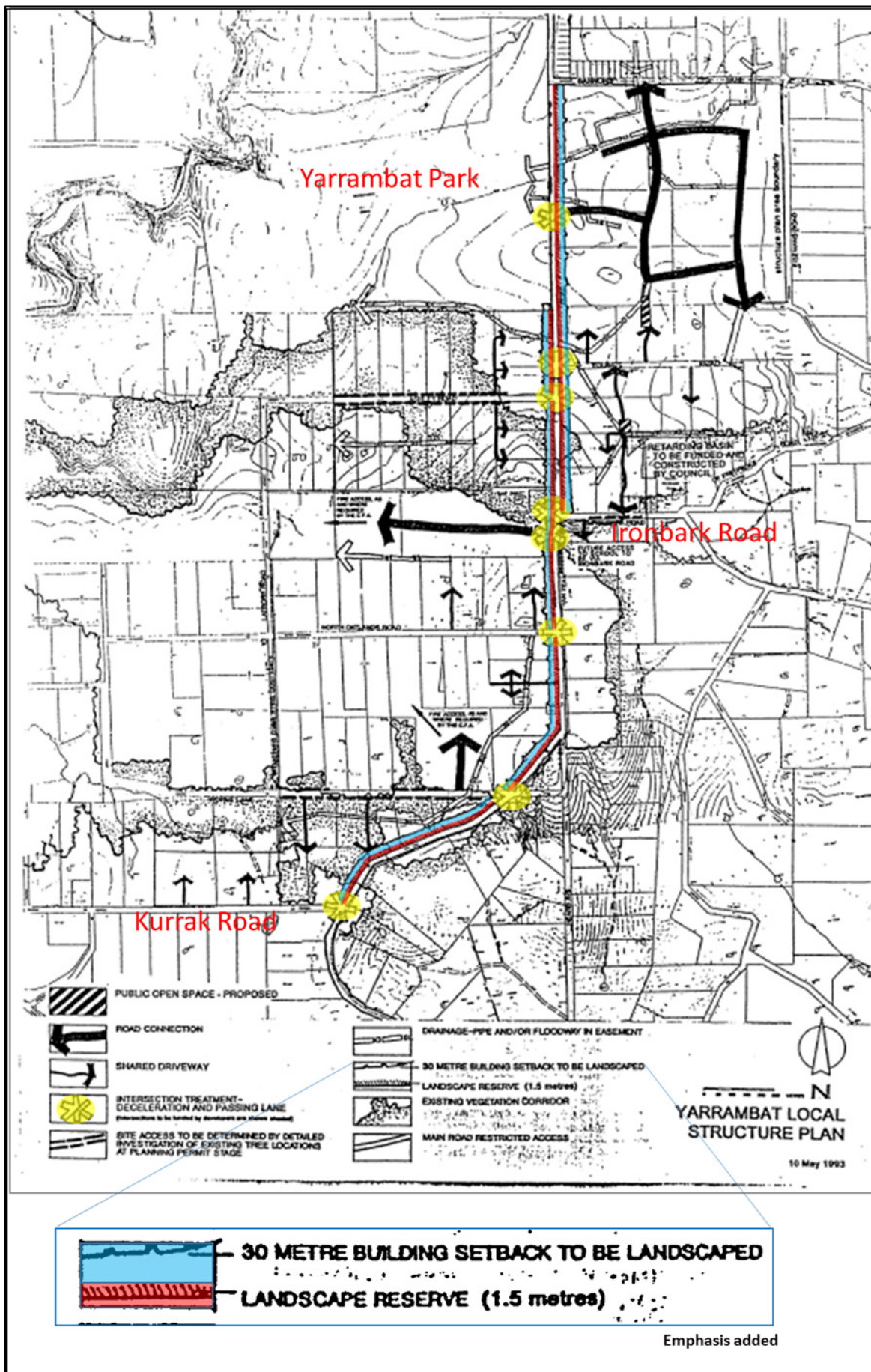
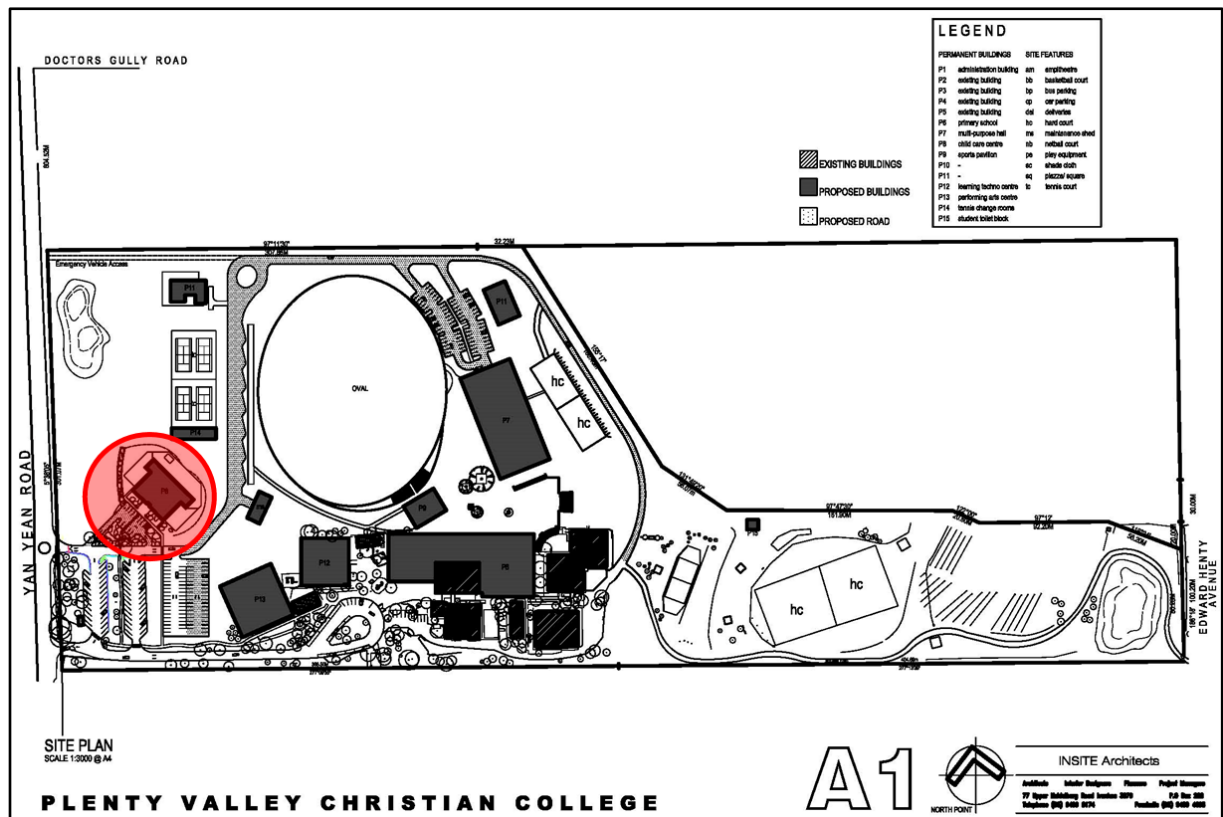


Figure 5.8 Yarrambat Local Structure Plan

5.6.5.2 PLENTY VALLEY CHRISTIAN COLLEGE MASTER PLAN, MARCH 2008

The Master Plan has been prepared by the Plenty Valley Christian College and was incorporated into the NPS in 2009 via Amendment C48. It sets out the vision for the site and timelines for development. Development of the site was planned to occur in several stages – Stages 1–5 prior to 2010, Stage 6 prior to 2020 and Stage 7 beyond 2020. Future development includes Stage 2 of the Primary School building with an indicative completion date of 2020 as well as a Sports pavilion, caretaker’s residence, learning technology centre and performing arts centre.

Recent aerial photography shows that access driveways to Yan Yean Road and car parking along the property boundary have been developed in accordance with the endorsed Master Plan. A child care centre has recently been completed near Yan Yean Road as shown highlighted red in Figure 5.9 (*our emphasis*). Built form remains set back from the road and no vehicle access is provided directly from Yan Yean Road. Any future development along the boundary to Yan Yean Road will need to be developed generally in accordance with the Master Plan and with regard to the proposed Yan Yean Road intersection.



Source: Insite Architects

Figure 5.9 Plenty Valley Christian Collage Master Plan

5.6.5.3 YARRAMBAT PARK MASTERPLAN 2012

NSC has adopted the *Yarrambat Park Masterplan 2012* with the aim to develop a plan that can be used to guide the future development of the park and golf course having regard to user and community aspirations while enhancing the natural character and ambience of the park. This Masterplan is a reference document within the NPS and provides an indicative site layout plan as shown in Figure 5.10 below. The development of the masterplan directly relates to Strategy 5 of the Nillumbik Recreation Strategy which seeks “*increased access to facilities*”. It is acknowledged within the masterplan that Yan Yean Road will need to be widened in the future and that the design of the golf course will have to take this into consideration. Key concerns relate to road safety associated with the proximity of the road to the tenth hole.



Figure 5.10 Yarrambat Park Masterplan 2012

5.6.5.4 OTHER STRATEGIC DOCUMENTS

Additional strategic documents prepared by NSC are detailed within Table 5.2.

Table 5.2 NSC strategic documents

NILLUMBIK SHIRE COUNCIL STRATEGIC DOCUMENTS	
<i>Heritage Strategy 2011</i>	The NSC <i>Heritage Strategy 2011</i> is a support document to the MSS and seeks to provide for the identification of sites of Aboriginal cultural significance and the appropriate level of management in consultation with the local Aboriginal community. The <i>North West Nillumbik Heritage Study, 2016</i> identifies two prominent mature River Red gum trees are located on the northeast corner of the Yan Yean and Doctors Gully Road intersection and protected by a Heritage Overlay (HO191). The study also identifies 920 Yan Yean Road as historically significant. Subsequently, NSC have drafted a heritage citation and a draft Heritage Amendment (Draft C104) for the asset. The draft citation provides a Statement of Significance for the Doreen Post Office and General Store and confirms “ <i>it is of local historic, aesthetic and social significance to the Shire of Nillumbik</i> ”. This has not formally been approved by Council and NSC has confirmed (September 2019) that this has been deferred until 2020.
<i>Nillumbik Trails Strategy 2011</i>	The <i>Nillumbik Trails Strategy 2011</i> is a support document to the MSS which will guide the planning and decision making in the provision of recreation trails. Whilst the document focuses on regional trails including the Aqueduct Trail and Diamond Creek Trail to the east of the project area, the Strategy outlines a vision that NSC “ <i>will be recognised for the quality and diversity of its trail network. The network will be developed and managed sustainably while ensuring economic, health and wellbeing benefits are provided to the community</i> ”. The Project intersects with the PGPR and Yarrambat Park which is a popular horse riding trail in NSC. The Project will incorporate SUP and a footpath along the length of the Project as well as consider appropriate crossings for pedestrians, cyclists, horses and riders to enhance the existing trail network in line with the Strategy.
<i>Nillumbik Green Wedge Management Plan 2019</i>	The <i>Green Wedge Management Plan 2019</i> is a support document to the MSS and identifies rural land use and capability and provides guidance in developing a sustainable direction for the non-urban areas of the municipality. The Plan highlights that car dependency in the area will continue as a result of the dispersed population, large quantity of people travelling outside the municipality for employment and services, and relative lack of public transport. The Project is in line with the Plan and will ensure that there is the appropriate transport infrastructure with sufficient capacity to support existing and planned growth of residential, employment and supporting land uses in existing and urban and green wedge areas including Yarrambat and Doreen.

NILLUMBIK SHIRE COUNCIL STRATEGIC DOCUMENTS	
<i>Nillumbik Housing Strategy 2001</i>	The Housing Strategy 2001 (currently under review) which is a support document to the MSS allows for “future housing development in the undeveloped GRZ1 and LDRZ in Yarrambat and Plenty. There is also potential for additional development to occur in existing built up areas on vacant sites or as additional dwellings on existing sites”. The Project supports the Housing Strategy by addressing current gaps in transport infrastructure within the outer suburbs and urban growth areas, and thereby providing additional capacity to cater for population growth and enabling the efficient movement of goods and people to their destinations.
<i>Open Space Strategy 2005</i>	The MSS identifies the importance of regional recreation resources which are serviced by Yan Yean Road including PGPR and Yarrambat Park. One of the listed recommendations within the <i>Nillumbik Open Space Strategy 2005</i> is to “liaise with Parks Victoria and encourage the extension of the trail network along the Plenty River from Greensborough to Yarrambat Park, through the Plenty Gorge Parklands”. It also identifies that “opportunities should be created to provide dedicated bicycle lanes along major roads”, including Yan Yean Road.
<i>Yarrambat Issues and Options Paper, 2014</i>	The <i>Yarrambat Issues and Options Paper</i> is a background report to the community’s request the Minister for Planning to recognise Yarrambat as a township and to implement a town boundary via application of the Development Plan Overlay (DPO). Whilst the Minister has not authorised the preparation of the PSA (refer to section 5.7.1.3), the paper highlights Yan Yean Road carries a high level of traffic from growth areas to the north, local residents must travel to meet their retail and service needs and planned population growth will exacerbate these issues. The Paper highlights a number of recommendations including the provision of right turn lanes along Yan Yean Road at Ashley Road, North Oatlands Road and Worns Lane and identify, prioritise and implement upgrades to bus stops.

5.7 FUTURE CHARACTER AND LAND USE CHANGE

A review of relevant PSAs and planning permits has been undertaken to identify future character or land use change.

5.7.1 PLANNING SCHEME AMENDMENTS

5.7.1.1 AMENDMENT C67 – YAN YEAN ROAD DUPLICATION (DIAMOND CREEK ROAD TO NORTH OF KURRAK ROAD)

Amendment C67 to the NPS was gazetted in 2012 and sought apply PAO1 to land along Yan Yean Road between Diamond Creek Road to north of Kurraak Road; introduce the incorporated document *Yan Yean Road Duplication (Diamond Creek Road to north of Kurraak Road) May 2012* under the Schedule to Clause 52.03 to enable road duplication works to occur, and amend the Schedule to Clause 81.01 to reference the new incorporated document. This project comprises the Yan Yean Road Stage 1 Upgrade, the northern extent of which connects into the project. The PAO1 applied to the land under Amendment C67 now applies to the land at the south of the alignment for this project. This amendment coupled with the proposed GC92 amendment will provide for a duplicated, 4 lane road, from Diamond Creek Road in the south to Bridge Inn Road in the north.

5.7.1.2 AMENDMENT C201 – MERNDA RAIL EXTENSION PROJECT

The MREP was incorporated into the WPS via Amendment C201. The *Mernda Rail Extension Project Incorporated Document, October 2016* (Amended November 2016) allowed for road and railway works and associated infrastructure. The MREP was completed in 2018 and includes the addition of three new stations, over 2,000 carparks, new cycling and pedestrian facilities, and grade separation at road intersections to eliminate the need for any level crossings. It also proposed to alter bus routes to align with the MREP, enhance feeder services from local growth areas and the frequency of services. This includes the local services such as the route along Yan Yean Road as illustrated in Figure 5.11.

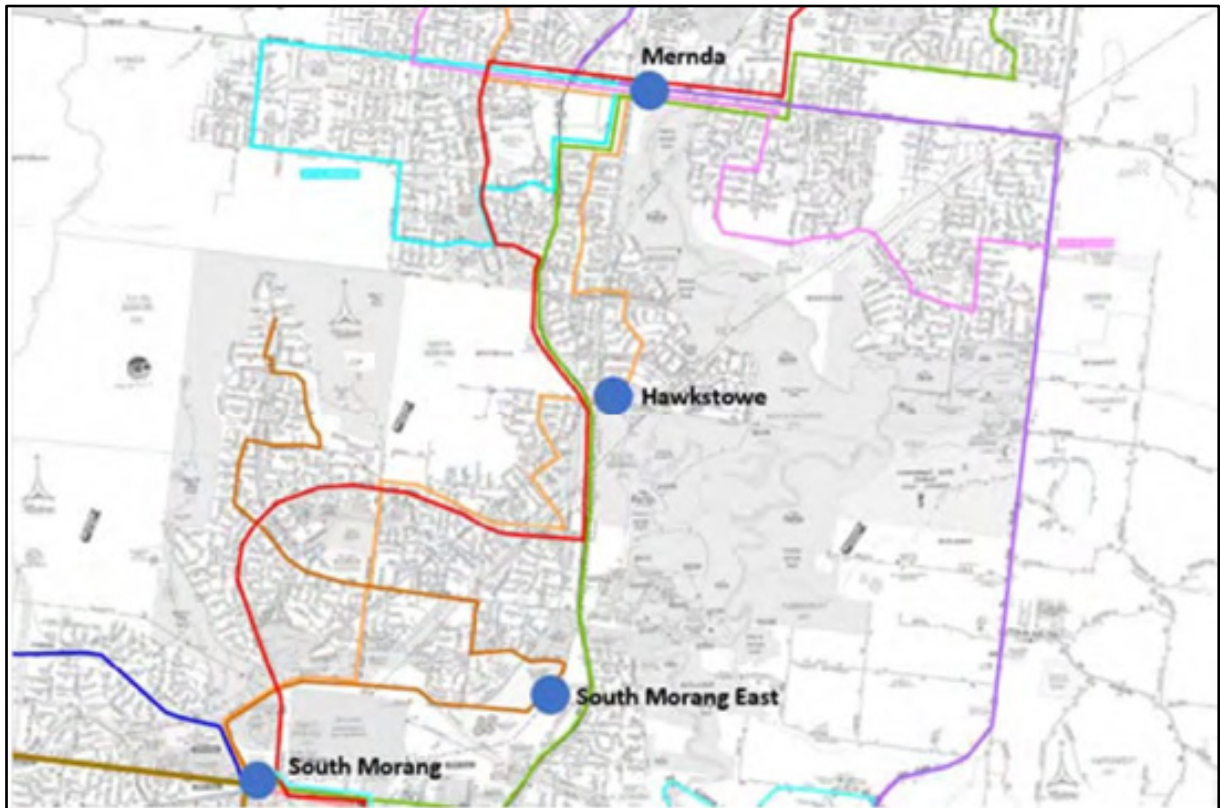


Figure 5.11 Bus routes connecting to the MREP

5.7.1.3 YARRAMBAT TOWNSHIP PLAN

At its Ordinary Meeting on 22 March 2016 Council resolved to commence an amendment to vary the NPS to formalise the Yarrambat Township. Council did so after considering a recommendation from the Yarrambat Township Advisory Group for a proposed township boundary and concept plan for Yarrambat (*Yarrambat Issues and Options Paper*, 2014). In June 2016 Council applied to the Minister for Planning to authorise an amendment to the NPS to formalise the Yarrambat Township by applying a new township boundary and inserting a new schedule to the scheme – DPO8 to all land within the proposed boundary. The DPO8 would require the preparation of a detailed development plan and that all future buildings, works and subdivision be in accordance with the plan.

In 2017, on the advice of the Minister for Planning, Council made the decision to proceed with preparing a detailed Yarrambat Township Plan, but modify the Project scope. The Minister for Planning advised that the increase to housing density outside the Urban Growth Boundary is inconsistent with State Planning Policy. Through previous public engagement, there is broad consensus to continue to explore the possibilities of some development in Yarrambat, whilst protecting its valued township and rural character.

In July 2018 Council resolved to investigate a staged approach to the preparation of township planning that involves two stages:

- Stage 1: A public realm and beautification plan
- Stage 2: Investigate with the State Government the possibility of a small scale commercial centre for the township.

Council and DELWP officers explored the potential rezoning of land to allow for a small scale commercial centre. DELWP have now indicated that rezoning for the purpose of a small commercial centre development will not be supported and requested that Council abandon the amendment.

At its Ordinary Meeting on 29 October 2019 Council concluded that the policy position held by the DELWP has strengthened to clearly prevent new housing and commercial development outside of the urban growth boundary. This made the intent of the amendment no longer viable and, accordingly, directed that the amendment be abandoned.

5.7.2 *PLANNING PERMITS*

A request has been made to Council to confirm if any recent planning permits have been issued along Yan Yean Road. A historical review of planning permits issued in the last three years identified the following permits of relevance:

5.7.2.1 6 YOUNGS ROAD, YARRAMBAT

Planning Permit No. 205/2017/01P approved on 04 October 2018 at VCAT for the Use and Development of the Land for a Childcare Centre for 80 Children. An endorsed plan has been requested from NSC to identify whether the Project will impact on the use and development of this site.

A permit (Council Ref: 10/ALT/2019) to alter the existing septic system was granted by NSC on 20 May 2019. The Victorian Building Authority issued a Compliance Certificate to allow the occupier to use 790 square metres of the land for subsurface irrigation. The endorsed plans provided by NSC illustrate a 10 metre buffer along the Yan Yean Road boundary, however minimal setback has been provided for the irrigation area.

6 RISK ASSESSMENT

The residual environmental risks identified for planning and land use are provided in Table 6.1. The residual risk ratings consider the standard controls and proposed EPRs. The proposed EPRs are set out in Table 8.1 in Section 8.

Table 6.1 Risk assessment – planning and land use risks

RISK NO.	ASPECT	IMPACT PATHWAY	MITIGATION MEASURES TO INFORM ENVIRONMENTAL PERFORMANCE REQUIREMENT	EPR	RESIDUAL RISK RATING
SITE ESTABLISHMENT					
11	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	<p>The Project must be designed and constructed to:</p> <ul style="list-style-type: none"> — minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the following land uses: <ul style="list-style-type: none"> — parks and reserves — other sensitive land uses such as educational facilities — recreational and community facilities — residential properties — commercial and industrial sites. — consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable — consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR LU1	Significant

RISK NO.	ASPECT	IMPACT PATHWAY	MITIGATION MEASURES TO INFORM ENVIRONMENTAL PERFORMANCE REQUIREMENT	EPR	RESIDUAL RISK RATING
EARTHWORKS					
31	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	<p>The Project must be designed and constructed to:</p> <ul style="list-style-type: none"> — minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the following land uses: <ul style="list-style-type: none"> — parks and reserves — other sensitive land uses such as educational facilities — recreational and community facilities — residential properties — commercial and industrial sites — consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable — consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR LU1	Significant

RISK NO.	ASPECT	IMPACT PATHWAY	MITIGATION MEASURES TO INFORM ENVIRONMENTAL PERFORMANCE REQUIREMENT	EPR	RESIDUAL RISK RATING
CIVILS AND STRUCTURES					
51	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	<p>The Project must be designed and constructed to:</p> <ul style="list-style-type: none"> — minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the following land uses: <ul style="list-style-type: none"> — parks and reserves — other sensitive land uses such as educational facilities — recreational and community facilities — residential properties — commercial and industrial sites — consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable — consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR LU1	Significant

RISK NO.	ASPECT	IMPACT PATHWAY	MITIGATION MEASURES TO INFORM ENVIRONMENTAL PERFORMANCE REQUIREMENT	EPR	RESIDUAL RISK RATING
REINSTATEMENT					
71	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation.	<p>The Project must be designed and constructed to:</p> <ul style="list-style-type: none"> — minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the following land uses: <ul style="list-style-type: none"> — parks and reserves — other sensitive land uses such as educational facilities — recreational and community facilities — residential properties — commercial and industrial sites — consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable — consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR LU1	Medium
OPERATIONS					
91	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation.	<p>Mitigation measures have been applied during the design and construction phases of the Project and use of the existing road is expected to continue. As such, the residual risk of impact on land use during operation of Yan Yean Road is considered to be low.</p> <p>Where permanent land acquisition is unavoidable:</p> <ul style="list-style-type: none"> — early and consistent consultation with affected land owners and tenants must occur — compensation for interests in acquired land must be assessed in accordance with <i>Land Acquisition and Compensation Act 1986</i>. 	EPR LU2	Low

RISK NO.	ASPECT	IMPACT PATHWAY	MITIGATION MEASURES TO INFORM ENVIRONMENTAL PERFORMANCE REQUIREMENT	EPR	RESIDUAL RISK RATING
MAINTENANCE					
111	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation.	<p>Mitigation measures have been applied during the design and construction phases of the Project and use of the existing road is expected to continue. As such, the residual risk of impact on land use during operation of Yan Yean Road is considered to be low.</p> <p>Where permanent land acquisition is unavoidable:</p> <ul style="list-style-type: none"> — early and consistent consultation with affected land owners and tenants must occur — compensation for interests in acquired land must be assessed in accordance with <i>Land Acquisition and Compensation Act 1986</i>. 	EPR LU2	Low

7 IMPACT ASSESSMENT

This section of the report addresses potential planning and land use impacts and provides a detailed consideration of the land acquisition impacts, construction (temporary) impacts and operational (permanent) impacts. The potential land use and planning impacts have been informed by the risk pathways identified in Section 6. The impact assessment also identifies potential benefits and opportunities to land use the Project is expected to generate.

7.1 CONSTRUCTION IMPACTS (TEMPORARY)

Land use impacts during construction are generally limited in duration and extent and associated with activities that can be inconsistent with established land use. Uses can be impacted by the temporary occupation of land for the purpose of construction. The potential land use impacts during construction are discussed below.

7.1.1 LAND USE

7.1.1.1 USE OF THE ROAD CORRIDOR

The primary purpose of Yan Yean Road is to operate as a movement corridor, providing access for vehicles, cyclists and pedestrians. During construction the road reserve will be used for temporary construction and laydown areas; temporary site offices, relocation and construction of minor utility installations and traffic diversions. Therefore, the road will be used in a manner inconsistent with the use of a road as a movement corridor, resulting in land use impacts. Management measures have been identified within Technical Report D – Social Impact Assessment, Technical Report A – Traffic Impact Assessment and Technical Report G – Landscape Strategy to address these land use impacts.

Impact mitigations for use of the road corridor are described in Section 1.

7.1.1.2 UTILITIES

The construction of the Project will require the relocation of existing utility services such as water, electricity and telecommunications within the road reserve or on land to be acquired. The required relocation of utilities would allow the Project to be constructed in a safe and efficient manner. Utility services are commonly located underground within road corridors to avoid impacts to private properties, to the greatest extent practicable. However, there may be a need to relocate drainage, water or sewer mains within private properties from land to be acquired. The extent of relocation has not yet been established and with appropriate mitigation measures the relocation of existing utility services would not result in a change to or an impact on existing land uses.

Early consultation has been undertaken with Melbourne Water (MW) and YVW to identify any existing assets or proposed projects that interface with Yan Yean Road. Discussions with YVW has ensured the Project boundary does not impact on YVW's current rising sewer main project (permit 428/2018/10P – refer to Section 7.5.2) which runs parallel to Yan Yean Road from Jorgensen Avenue to Raoul Court and then crosses Yan Yean Road to Ironbark Road. The majority of this project will be underground bored.

Early consultation with MW has been undertaken to identify opportunities to integrate drainage into existing schemes and to implement landscaping enhancement. This will be confirmed at detailed design stage and as such, the impact on land use change has not been assessed within this report. Further details are provided in Technical Report L – Surface Water Impact Assessment.

7.1.2 AMENITY IMPACTS

During construction there will likely be amenity impacts associated with the Project including disruption to access, air quality, noise, visual and an increase in construction traffic. These amenity impacts have been assessed in more detail within each of the technical reports listed in Section 1 and mitigation measures identified.

The potential amenity impacts are not expected to result in a change to, or an impact on, existing land uses. Subsequently, mitigation measures are not identified or recommended.

7.2 CONSTRUCTION MITIGATIONS

7.2.1 USE OF THE ROAD

To minimise the impacts on the use of the road during construction it is recommended that the duration and extent of occupancy of each section of the Project be managed. During periods of occupation alternative access will be made available to maintain movement along the corridor and to avoid restricting access to public and private land. These measures coupled with early and consistent feedback to road users on the impact of construction on their journey will allow for the road to be maintained as a movement corridor during construction.

At the end of the construction period the landscape strategy will be implemented and legal access to service lanes or local road access will be reinstated.

7.2.2 UTILITIES

To minimise the impacts on utilities appropriate construction methodologies will be developed as part of the CEMP including underground boring to minimise impacts on existing and future assets. During construction, engagement with potentially affected residents and businesses during utility relocation works will be undertaken early to ensure that users are aware of upcoming disruptions to allow them to make alternative arrangements. In addition, any disruption to services will be managed to provide for continuation of land uses.

7.3 OPERATIONAL IMPACTS (PERMANENT)

The use and development of a road where there is an established secondary arterial road, zoned for the purposes of a road is consistent with the established land use and identified land use policies. Potential land use impacts during operation therefore relate to the permanent occupation/acquisition of land outside of the existing road corridor and the impact of this on the established land uses. Land use change brought about by the Project may potentially result in inconsistencies with planning policy and strategic plans across the Project.

7.3.1 LAND USE PLANNING POLICY

7.3.1.1 USE OF THE ROAD CORRIDOR

The Project is generally consistent with, and implements *Plan Melbourne*, the PPF and LPPF of the WPS and NPS and strategic planning documents as outlined in Section 4.2.1, 4.2.2, 5.6 and Appendix C and Appendix D. In particular:

Plan Melbourne – A key outcome of *Plan Melbourne* is that “*Melbourne has an integrated transport system that connects people to jobs and services and goods to market*”. Two of the key directions supporting this outcome is to “*transform Melbourne’s transport system to support a productive city*” and to “*improve transport in Melbourne’s outer suburbs*”. These have associated policies relating to improving arterial road connections across Melbourne, specifically Policy 3.2.1 “*improve roads in growth areas and outer suburbs*”. Policy 1.2.1 seeks to “*support the development of a network of activity centres, linked by transport*”. The Project will support

the rapid growth of Mernda/Doreen in the North Growth Corridor and improve regional arterial road network capacity and connectivity to the M80, La Trobe NEIC, Melbourne Airport and the Northern Industrial Precinct. The Project will contribute to an improved outer suburbs arterial road network, leading to greater efficiencies, improved productivity, and greater access to jobs, services and markets. Policy 3.1.6 focuses on “*supporting cycling for commuting*” by establishing strategic cycling corridors. The Project will introduce off road cycling facilities linking into the future Bridge Inn Road SUP and south into the Yan Yean Road Stage 1 SUP. These infrastructure improvements will support the development of the PBN within the North Growth Corridor.

The MSP recognises that designing quality urban and open spaces are essential to increasing and improving connectivity of active transport to an integrated public transport network. As part of the delivery of the MREP, a bus interchange will be built at Mernda Station to service surrounding area. In order to allow reliable and regular bus services, the MSP recommends that Yan Yean Road needs to be duplicated. Bus routes are encouraged to follow arterial roads and link key destinations such as activity centres and the MREP to residential areas. The *Doreen Recreation Reserve Master Plan 2014* and *Yarrambat Local Structure Plan 1993* carry the expectation of the future widening and intersection upgrade of Yan Yean Road and Bridge Inn Road. These documents outline and provide for extra land requirements and setbacks for the Project. The Project supports the objectives outlined in the MSP and is generally consistent with the *Doreen Recreation Reserve Master Plan 2014* and *Yarrambat Local Structure Plan 1993*, as well as the proposed MSP cross section for Yan Yean Road which is outlined in Figure 5.5.

The PPF, and more particularly Clause 18 (Transport) – Seeks to ensure an integrated and sustainable transport system that provides access to social and economic opportunities, facilitates economic prosperity, contributes to environmental sustainability, coordinates reliable movements of people and goods, and is safe. The Project will improve arterial road capacity enabling improved sustainable transport connections (bus services, cycling and walking) to the Mernda Train Station and bus interchange and future MTC. Full duplication of Yan Yean Road will facilitate improved connections to the M80 which provides regional access to employment and services.

MSS – Clause 21.02 – Municipal Profile (WPS) identifies improving the capacity and connectivity of east-west and north-south transport routes as a priority to support the significant growth within Mernda/Doreen, Epping North and South Morang. The Project is consistent with and implements this clause improving arterial road capacity and connectivity particularly to support the significant growth being experienced within Doreen.

Clause 21.11 Transport (WPS) – A key focus for WCC is the development of an integrated transport system and a key objective of Clause 21.11-1 Integrated Transport is “*to establish an efficient, interconnected multi-modal transportation system which increases the level of accessibility and choice within and beyond the City of Whittlesea*”. Roads are a part of this integrated transport framework and the Project will enable more efficient bus services and provide a continuous SUP along the western side of the road. Supporting integrated transport is the need to provide improved transport choice with a shift towards more sustainable modes of travel. Objective 1 of Clause 21.11-2 Sustainable Modes of Travel is “*to improve transport options and accessibility outcomes for residents*”. The Project will also facilitate improved active, public and private vehicle access to metropolitan and district open space.

MSS – Clause 21.03 Municipal Profile and Key Influences (NPS) identifies that a high level of car usage and population growth increases travel demand. The need for the Project is to address the increase in travel demand resulting from the key influences outlined within the NPS. Clause 21.04 – Strategic Framework (NPS) recognises the importance of the arterial road network to connect residents to employment and services outside of the green wedge. The Project directly contributes to increasing the capacity of Yan Yean Road to improve access to the growth area of Mernda/Doreen and to facilitate improved arterial connections from the green wedge township of Yarrambat to the residential areas of Plenty and Diamond Creek and the M80. Clause 21.05-1 Settlement and Housing reinforces the existing settlement pattern with a focus on directing future growth into existing zoned areas within Yarrambat and Plenty. The Project will support additional growth within these areas. Clause 21.05-3 Environment, Conservation Landscape seeks to “*Protect and enhance roadside vegetation through the implementation of the Nillumbik Shire Council Roadside Management Plan 2012*”. This is addressed within Technical Report B – Flora and Fauna Impact Assessment and Technical Report G – Landscape Strategy.

The Project is generally consistent with the intended and strategic directions considered for the land use in the study area and implements the identified policies as above. No mitigation measures are considered required or recommended.

7.3.2 CHANGE IN LAND USE RESULTING FROM LAND ACQUISITION

7.3.2.1 OVERVIEW OF LAND ACQUISITION

The Project requires the acquisition of private, public and council land. At the time of writing, the Project requires full acquisition of 21 linear road reserve parcels, and partial acquisition of 75 parcels.

EXISTING PUBLIC ACQUISITION OVERLAY

During the planning for Yan Yean Road Upgrade Stage 1, land was identified for the future widening of Yan Yean Road and Amendment C67 to the NPS applied PAO1 (acquisition by Roads Corporation for road purposes) over this land.

Eight parcels were acquired as part of the Stage 1 upgrade, which fall within the Stage 2 project area. These parcels were acquired by the DoT. The tie in between Stage 1 and 2 will also result in additional land being acquired from 3\LP43702. This parcel is detailed on the PAO Maps provided at Appendix H.

A summary of the affected land parcels is provided within Appendix I and the impact on the ongoing use of land is provided at 7.3.2.2.

PROPOSED PUBLIC ACQUISITION OVERLAY

At the time of writing, there are 96 parcels that are not currently subject to a PAO. For parcels without existing inter-government acquisition arrangements, a PAO would be required to be applied to the land. These parcels are detailed on the PAO Maps provided at Appendix H.

Of the land to be acquired that is not currently subject to a PAO, ownership details are as follows:

Table 7.1 Proposed land acquisition for the Project

LANDOWNER STATUS	NO. OF PARCELS
Council	28 (26 NSC and 2 WCC)
Private	60
Public Authorities/State	8

Discussions with respective land owners regarding acquisition of the land is being undertaken and a summary of the sites to be acquired are provided in Appendix I.

A review of all CoT along the Project alignment has been undertaken and provided at Appendix G.

7.3.2.2 IMPACT ON THE ONGOING USE OF LAND

RESIDENTIAL

590 YAN YEAN ROAD

The Project will result in the need to acquire land along the frontage of 590 Yan Yean Road and will result in the demolition of an existing residential building. This property contains an unoccupied dwelling and an additional new dwelling, which will not be impacted by acquisition. The construction of the new dwelling was subject to Planning Permit No. 51/1998/01P. The Project will result in demolition of the original unoccupied house and subsequently result in a change in land use. As the house is currently unoccupied it is understood that the Project will not displace residents and the remaining land will still be used for residential purposes.

Mitigations for 590 Yan Yean Road are discussed in Section 7.4.1.

815-835 YAN YEAN ROAD

As outlined in Section 5.6.4.1, land to west of Yan Yean Road from Cookes Road to Jorgensen Avenue is covered by the *Mernda Local Structure Plan – Part 1 1994* and the MSP. In addition, there are a number of development plans which have been approved for sites within the MSP area including the *Garden Road Development Plan*, and *815–835 Yan Yean Road Development Plan*. The impact of land acquisition on land identified for future residential development is outlined below.

Development Plan 815–835 Yan Yean Road, 2017 provides for the future residential subdivision of this land and construction has commenced on the two northern lots. The *Mernda Strategy DCP 2008* (Mernda DCP) provides for per hectare contributions for infrastructure and open space within Precinct 2B. The Development Plan identifies future road widening at the intersection of Yan Yean Road and Orchard Road to allow for the potential future signalisation of this intersection. The Development Plan states that the exact extent of the road widening is to be determined in consultation with Council and VicRoads, at the time of the planning permit application for the subdivision. At this time, it is understood that there has been no subdivision permit lodged for this site but this is being confirmed with WCC.

Figure 7.1 indicates the extent of the land required to facilitate the upgrade and highlights that the Development Plan has made no allowance for the future widening of Yan Yean Road along the site's eastern extent. The road widening will result in loss of approximately 1,500 sqm of land from the total 9.8 hectares which represents 1.5% of the total landholding and a minor change to the land budget (refer to risk assessment and risk R-PLU2). The Mernda DCP states that “the number of developable hectares is based on land budgets associated with the precinct plans contained in the Mernda Strategy Plan. The per hectare contributions payable under the Mernda DCP will not be amended to respond to minor changes to land budgets that may result from the Development Plan process”. Therefore, it is reasonable to suggest that the Project will not impact on the land budget calculations of the Mernda DCP or prejudice the implementation of this plan. WCC has not yet confirmed this interpretation of the Mernda DCP.



Figure 7.1 Project impact on the MSP (extract from Figure 6.4) and Development Plan 815-835 Yan Yean Road, 2017

The minor acquisition of land along the frontage of the site within an identified tree reserve will not prejudice the orderly use and development of this land as intended by the MSP or the Development Plan. However, the future subdivision application will need to take into consideration the additional land requirements for road widening and provide an appropriate landscape buffer between future residential development and the Project.

Mitigations for 815–835 Yan Yean Road are described in Section 7.4.1.

YARRAMBAT TOWNSHIP

As outlined in section 5.6.5.1 land between Yarrambat Park and Kurrak Road is within the boundary of Yarrambat which is intersected by Yan Yean Road. In accordance with the LSPs for this area, large lots were established and built form is well set back from the road (in some cases planning established a 30 metre setback from the road) and where possible direct access was restricted. Potential residential land use impact predominately constitutes acquisition of land from residential properties. The majority of properties are large rural-residential allotments and acquisition is limited only to small slivers of land on frontage of their lots and driveways at the interface with Yan Yean Road. Given the extent of acquisition, particularly in relation to the lot size, the acquisition will not impact on the ongoing existing use of this land. No other mitigation measures are considered required or recommended. Assessment of landscape and visual values are detailed further in Technical Appendix G – Landscape Strategy.

COMMERCIAL

YELLOW BRICK ROAD COMMERCIAL CENTRE

South of Bridge Inn Road and on the western side of Yan Yean Road, the land is within Precinct 2B of the MSP and the *Garden Road Development Plan*. The *Garden Road Development Plan* is shown in Figure 7.2 which provides for the widening of Yan Yean Road. Aerial photography confirms that the built development on the corner of the site has been completed and is well setback from the intersection with a significant reserve separating the commercial uses and onsite parking facilities from the intersection. Whilst the project boundary abuts this parcel, the proposed works are located entirely in the grass reserve and link into the existing SUPs. The existing access to the commercial development via Yellow Brick Road will be retained.

The Project will provide a SUP to the south of Bridge Inn Road which links into the SUP proposed for the Bridge Inn Road Upgrade. This will require works within the Ausnet easement identified within the Development Plan. The Development Plan requires that the local road shown running parallel to the easement must be contained wholly outside the easement. The Project will not impact on any existing development or businesses within this development. A request to WCC has been made to obtain copies of the proposed plans for the pending subdivision permit at this site for 37 lots.

Mitigations for Yellow Brick Road Commercial centre are described in Section 7.4.1.

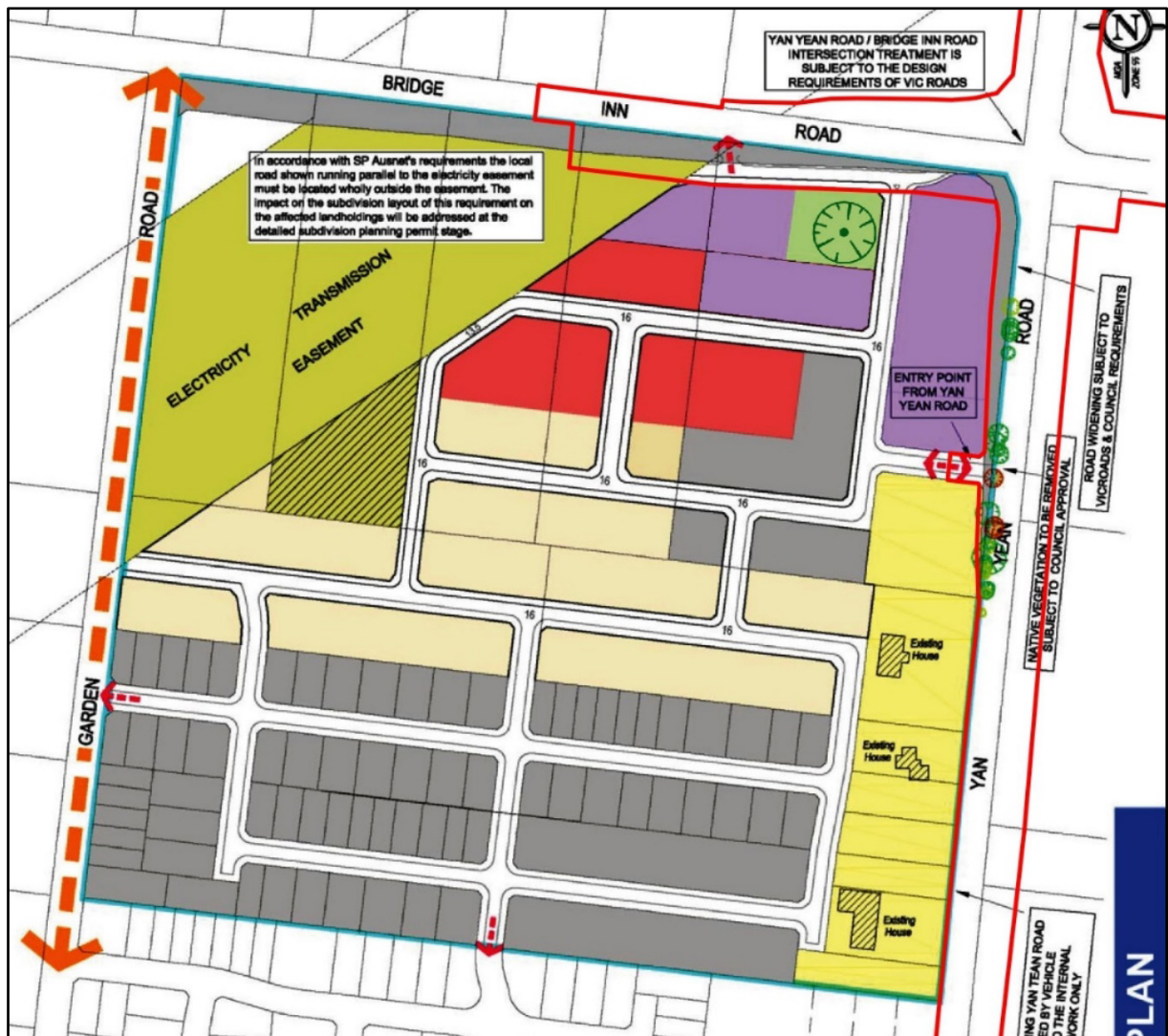


Figure 7.2 Project impact on Garden Road Development Plan

OTHER NON-RESIDENTIAL USES

Other non-residential uses which are impacted by acquisition include the Yarrambat Veterinary Hospital and the St Macarius Coptic Orthodox Church. The Yarrambat Veterinary Hospital is located on the west side of Yan Yean Road with direct access via Yan Yean Road. The hospital building is well set back from the road behind an existing open paddock. The Project requires the partial acquisition (2,534 sqm) of this frontage but will not impact on the viability or ongoing use of the paddock or Yarrambat Veterinary Hospital and as such will not result in a land use change. The St Macarius Coptic Orthodox Church is located on the west side of Yan Yean Road, with access from Ashley Road. To facilitate the Project land will need to be acquired along this frontage (750 sqm) but will not directly impact the church building, car parking and existing access. Acquisition of this land will not result in a change to land use or an impact on the existing use of this land, therefore no mitigation measures are proposed. Compensation for interests in acquired land will be assessed in accordance with the LA&C Act.

7.3.2.3 RURAL LAND USES

Land within the Green Wedge Zone and Rural Conservation Zone comprises a range of lots sizes from 2 hectares up to 60 hectares as outlined within section 5.5.4 and 5.5.5. Minimum subdivision area under Clause 35.04 Green Wedge Zone is 40 hectares and under Schedule 3 to the Rural Conservation Zone is 8 hectares. Within this area, fragmentation of lots has already occurred over time and there are a number of lots within this area that are well below the minimum subdivision area or else not of sufficient size to enable further subdivision. Therefore, the loss of land acquired along the frontage of these lots is unlikely to impact on the future development potential of this land or impact on the viability of the current use of the land (see Technical Appendix G – Landscape Strategy). It is recommended that early and consistent consultation with all affected landowners and those with interest in the land be undertaken and compensation for interests in acquired land will be assessed in accordance with the LA&C Act. No other mitigation measures are considered required or recommended.

7.3.2.4 OPEN SPACE

DOREEN RECREATION RESERVE

As highlighted in 0, the Doreen Recreation Reserve is subject to the *Doreen Recreational Reserve Masterplan 2014* and falls within precinct 2A of the *Mernda Strategy DCP 2008* (Mernda DCP). The Mernda DCP requires all development plans to provide open space in accordance with Map 4 of the DCP. Map 4 shows that the portion of the Doreen Recreation Reserve, under the transmission easement, as encumbered open space, with a small triangular portion, at the intersection of Bridge Inn and Yan Yean Roads, as active recreation. It is unclear whether this land is funded via the DCP. This land currently remains largely undeveloped and is not used for active recreation at the present time.

In developing the masterplan, WCC made assumptions about the land that would be required to facilitate the widening of the Bridge Inn Road and Yan Yean Road intersection. However, the design requires additional land beyond that originally identified. The design has been applied to the masterplan and is provided within Figure 7.3 below. This land was not included within the original land budget calculations of the Mernda DCP and as such loss of this additional land will not impact on land budget or financial contribution calculations. The Mernda DCP establishes an Open Space Equalisation Account to ensure that open space is shared equitably across all landholdings. This allows for cash contributions in lieu of open space provision.

Figure 7.3 shows that the land acquisition required to facilitate the Project will reduce the land available along the southern and eastern boundaries of the masterplan area. The Project is likely to result in the need to reconfigure or relocate the tennis court and oval, northwards. However, this is yet to be confirmed by WCC. The Masterplan 2014 identifies that there is “*potential area for future expansion of land reserve*” to the north of the masterplan area. Changes to the amount of land to be acquired from the Doreen Recreation Reserve (a total of 9156 sqm) will not change the intended use of the land by the MSP or the Masterplan 2014 and is unlikely to result in any permanent or long term disruptions to land use. It will, however, reduce the land area currently available and may require Council to amend the Masterplan 2014 accordingly.

The following mitigation measures are recommended but will need to be refined following discussion between MRPV and WCC:

- early and consistent engagement with WCC
- compensation under the LA&C Act
- consideration of a revision of the 2014 Masterplan.

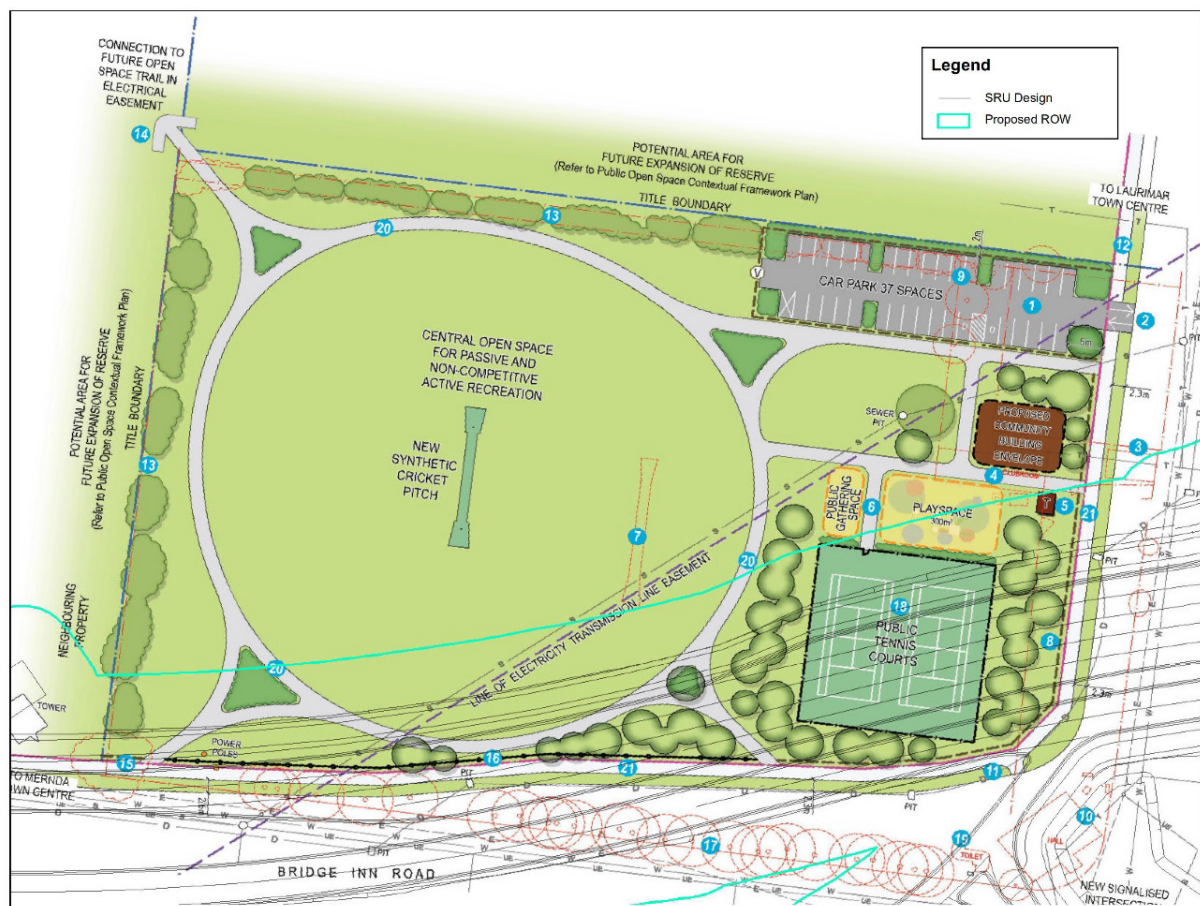


Figure 7.3 Project impact on Doreen Recreation Reserve Masterplan

PLENTY GORGE PARKLAND RESERVE

The PGPR is subject to the *Plenty Gorge Park Masterplan 2018* (Masterplan 2018) which includes the Yarrambat Picnic Area. Objective 1 of the Masterplan is to improve access and connections to the PGPR. The Masterplan 2018 does not allow for the future widening of Yan Yean Road but does recommend working with council to improve safety, access and connections with Yarrambat Park. To facilitate the Project, land will be acquired from the PGPR (467.46 sqm) along the Yan Yean Road frontage. This partial acquisition comprises a linear area east of the picnic area. Acquisition of this land will not impact on any structures or built form within the site and will not impact on the orderly planning of this area as intended by the Masterplan 2018. Acquisition of the land would occur via existing inter-government arrangements between DELWP and the DoT. No other mitigation measures are considered required or recommended. The potential opportunities and benefits of the Project are discussed in section 7.3.3.1 below.

YARRAMBAT PARK

Yarrambat Park is located south of the PGPR abutting Yan Yean Road. It is owned and managed by NSC and includes active recreation facilities, including an 18-hole golf course, driving range, archery range and a number of sporting and recreation clubs. The park is subject to the *Yarrambat Park Masterplan, 2012* (Masterplan 2012) in which, development is well set back from Yan Yean Road. In particular, the development potential for the golf course has accounted for future road widening. The Project requires the partial acquisition of land from within Yarrambat Park for road widening purposes. As with the PGPR, acquisition comprises a linear area directly adjacent to Yan Yean Road. This will not have an impact on any built form on site or the function of the park. However, it is noted that to protect safety of road users, measures will be required to address potential impacts from golf balls and arrows due to the proximity of the tenth hole and the archery range to Yan Yean Road. Netting, or other safety management measures, may be required to be installed along the boundary with

Yan Yean Road to prevent objects entering the road reserve. A safety assessment will be required and an appropriate management response will need to be considered by Nillumbik Shire Council in consultation with MRPV and DoT.

With the application of a PAO to this land via a PSA, acquisition of the land and compensation will be undertaken in accordance with the LA&C Act. A safety assessment should be undertaken in relation to the golf course and archery range to establish whether specific safety measures are required. No other mitigation measures are considered required or recommended. The potential opportunities and benefits of the Project are discussed in section 7.3.3.1 below.

7.3.2.5 EDUCATION

The Plenty Valley Christian College is located to the east of Yan Yean Road and is accessed via the roundabout at Orchard Road. Existing and proposed (*Plenty Valley Christian College Masterplan, 2008*) built from (including the newly constructed Child Care Centre) is well set back from Yan Yean Road. The Project requires acquisition of land fronting Yan Yean Road however; no structures are affected and impact is restricted to site access arrangements and onsite parking. The school has been consulted on these changes and compensation for interests in acquired land will be assessed in accordance with the LA&C Act. Works to onsite parking are outside of the project boundary and do not comprise part of this project at this time. This is yet to be confirmed by MRPV. No other additional mitigation measures are required or proposed.

7.3.3 ACCESS

The majority of acquisition comprises small slivers of driveways directly fronting onto Yan Yean Road which will change the current access arrangements to these land uses. The priority for the Project is to ensure that the new access regime will result in an appropriate level of road safety commensurate with the function of the road and number of trafficable lanes. These access changes are unlikely to result in a land use change, as alternative access arrangements including U-turn facilities, service roads or re-establishment of affected driveways onto an adjacent road will ensure that there will be no resultant loss in access. For local businesses reliant on passing trade, access has generally been retained. As outlined in Appendix G, NSC has entered into Section 173 Agreements restricting access via Yan Yean Road for land on its east side, with a preference towards gaining access via Youngs Road or its extension. In addition, at the Bridge Inn Road intersection, the service lane to Yellow Brick Road, will be reinstated to maintain access to the commercial centre. At 790 Yan Yean Road, the Project will reinstate one driveway to access the property following consultation held with the landowner. Furthermore, an extended service lane onto Yan Yean Road is provided for the residential properties located along Yan Yean Road south of Ashley Road.

Yarrambat Primary School is located to the east of Yan Yean Road, south of Ironbark Road. The reference design widens the road into land on the western side of the road to minimise acquisition of school land. However, the Project will alter the main vehicular access and, associated with these works, the onsite parking provisions. The school has been consulted on these changes.

Early and consistent consultation with landowners should be undertaken and compensation for interests in acquired land will be assessed in accordance with the LA&C Act. No further mitigation measures are required or proposed. Access changes are also assessed in the Technical Report A – Traffic Impacts Assessment and Technical Report D – Social Impact Assessment in relation to its traffic and social impacts.

7.3.3.1 BENEFITS AND OPPORTUNITIES

The Project provides a continuous SUP along the western side of Yan Yean Road and a separated footpath along the east. This will deliver improved active transport links along the PBN to key land uses such as Yarrambat Primary School, the Plenty Valley Christian College, Yarrambat Park and Yarrambat Horse and Pony Club, Werther Park, PGPR and Doreen Recreation Reserve which is consistent with State and local policy objectives.

In particular, the SUP at Werther Park has been designed to traverse through the park to minimise tree loss and link up with the existing path network. South of Werther Park, the SUP provides direct access to Yarrambat Park and the PGPR to the west. The Project is consistent with the objectives of *Plenty Gorge Park Masterplan 2018* improving connections from the local communities of Yarrambat, Doreen and Mernda to Yarrambat Park, Yarrambat Horse and Pony Club and other areas of PGPR. The Project achieves an objective of the *Nillumbik Open Space Strategy 2005* providing dedicated bicycle lanes along major roads. The SUP at Doreen Recreation Reserve is consistent with the Masterplan for the site and will connect into the SUP provided as part of the Bridge Inn Road Upgrade. The SUP continues north of the recreation reserve to allow future development in Doreen to connect to this network and in time, provide a connection to Yan Yean Reservoir (*Northern Regional Trails Strategy 2016*).

7.3.3.2 AMENITY

During the operation of the Project there will be amenity impacts associated with the Project including permanent access changes, air quality, noise, and an increase in traffic, cyclists and pedestrians. These amenity impacts have been assessed in more detail within each of the technical reports listed in section 1 and mitigation measures identified.

The potential amenity impacts are not expected to impact on land uses and therefore land use mitigation measures are not identified or recommended.

7.4 OPERATION MITIGATIONS

7.4.1 LAND ACQUISITION

7.4.1.1 590 YAN YEAN ROAD

The reference design has sought to avoid the removal of permanent structures/built form as far as practicable. The Project will require partial or full acquisition of 96 parcels. The removal of the residential building at 590 Yan Yean Road is necessitated by condition of their planning permit. The removal is therefore not considered a direct result of the Project. The new dwelling on this lot will not be impacted by the Project and the ongoing use of this land for residential purposes will not change.

Where permanent acquisition of a dwelling or private land is unavoidable, compensation for interests in acquired land will be assessed in accordance with the LA&C Act. Additional mitigation measures are recommended including early and consistent consultation with all affected landowners. These measures will maintain a Medium risk rating.

7.4.1.2 815-835 YAN YEAN ROAD

With the application of a PAO to this land via a PSA, acquisition of the land will be undertaken in accordance with the LA&C Act. These mitigation measures will reduce the initial medium risk rating to a residual low risk rating. No other mitigation measures are considered required or recommended.

7.4.1.3 YELLOW BRICK ROAD COMMERCIAL CENTRE

The reference design proposes to avoid or minimise impacts to existing commercial uses as far as practicable. MRPV has commenced discussions with WCC and landowners regarding acquisition and impact on future development of the Garden Road site. However, WCC has not provided the plans at this time to confirm impact on the proposed subdivision layout or provide direction on an appropriate mitigation response.

7.4.2 SAFETY

There is a potential safety risk associated with the proximity of the golf course and archery range within Yarrambat Park to the proposed road alignment. Netting, or other safety management measures, may be required to be installed along the boundary with Yan Yean Road to prevent balls or arrows entering into the road reserve. A safety assessment will be required and an appropriate management response will need to be considered by NSC in consultation with MRPV and Department of Transport.

7.5 FUTURE LAND USE AND DEVELOPMENT

A request was made to WCC and NSC for confirmation of current and recent permit applications and PSAs within the vicinity of the Project. At this time, no response has been received however, the following are considered relevant:

7.5.1 AMENDMENT C235 – BRIDGE INN ROAD (PLENTY ROAD, MERNDA TO YAN YEAN ROAD, DOREEN) UPGRADE PROJECT

Amendment C235 to the WPS amends the Schedule to Clause 45.12 Specific Controls Overlay of the WPS to insert SCO3 and a new Incorporated Document titled *Bridge Inn Road (Plenty Road to Yan Yean Road) Upgrade Project Incorporated Document, April 2019* to exempt all uses and development associated with the project from the need for a planning permit, subject to conditions. The amendment also applies Clause 45.01 Public Acquisition Overlay (PAO2) to land associated with the Bridge Inn and Yan Yean Road – Stage 2 Upgrade Projects and amend Planning Scheme Map No. 13PAO and insert a new Map No. 14PAO.

Amendment C235 was approved by the Minister for Planning in November 2019 and directly connects with the SCO of this Project and the PAO2 applied under Amendment C235 will impact on the PAO required to be applied for this Project. Lot 1\TP139445 will be partially acquired by Amendment C235. An additional area of this lot will be acquired for this Project. In addition, the SUP on the west side of Yan Yean Road proposed as part of this Project will connect with the SUP to be delivered on the south side of Bridge Inn Road under Amendment C235.

7.5.2 807 YAN YEAN ROAD

Planning Permit No.428/2018/10P for the “*Removal, destruction and lopping of vegetation (to facilitate the construction of a sewage transfer pipeline)*” at 807 Yan Yean Road was approved on 24 July 2019. This permit relates to the rising sewer project proposed by YVW. Of relevance to the Project, the pipeline includes an open cut adjacent to Yan Yean Road (west) from Jorgensen Avenue to Raoul Court. The pipeline is then proposed to be underground across Yan Yean Road to Ironbark Road. Engagement between YVW, Melbourne Water and MRPV is ongoing to ensure that neither project will result in detrimental land use impact.

8 ENVIRONMENTAL PERFORMANCE REQUIREMENTS

Table 8.1 lists the proposed Environmental Performance Requirements (EPRs) relevant to the planning and land use assessment.

Table 8.1 EPRs relevant to planning and land use

PERFORMANCE OBJECTIVE	APPLICABLE LEGISLATION, POLICY AND GUIDELINE	EPR CODE	RISK NO.	ENVIRONMENTAL PERFORMANCE REQUIREMENT	PROJECT PHASE
Effects on land use planning – Identify other potential adverse environmental effects of the project, such as on land use, and planning, and canvass an environmental management approach and performance measures to ensure any effects are identified and avoided, minimised or mitigated.					
Land use planning To minimise impacts on existing and proposed future land use	<i>Planning and Environment Act 1987</i> <i>Land Acquisition and Compensation Act 1986</i>	EPR LU1	11, 31, 51 & 71	Minimise land use impacts <ul style="list-style-type: none"> — The Project must be designed and constructed to minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the following land uses: <ul style="list-style-type: none"> — parks and reserves — recreational and community facilities — residential properties other sensitive land uses such as educational facilities — commercial and industrial sites. — Consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable and consistent with land zoning and the planning policy framework — Consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	Design and construction

PERFORMANCE OBJECTIVE	APPLICABLE LEGISLATION, POLICY AND GUIDELINE	EPR CODE NO.	RISK NO.	ENVIRONMENTAL PERFORMANCE REQUIREMENT	PROJECT PHASE
		EPR LU2	91 & 111	<p>Land acquisition</p> <p>Where permanent land acquisition is unavoidable:</p> <ul style="list-style-type: none"> — early and consistent consultation with affected land owners and occupiers must occur — continue one-on-one consultation with affected landowners and occupiers to outline the acquisition and compensation process, discuss changed access arrangements and provide clear timelines of proposed action — compensation for interests in acquired land must be assessed in accordance with <i>Land Acquisition and Compensation Act 1986</i>. 	Design and construction

9 CONCLUSIONS

This planning and land use impact assessment has been undertaken to determine the impacts of the Project on planning and land use. This assessment has identified the potential risks and impacts associated the Project including: land acquisition; access; land use change; consistency with planning policy, guidelines and plans. In turn, this assessment has informed management and mitigation measures to reduce the potential planning and land use risks associated with the Project.

9.1 EXISTING CONDITIONS

The Project is located within a predominantly low density residential and rural living area within the metropolitan Green Wedge. Yan Yean Road forms the eastern boundary of the Whittlesea Growth Corridor, between Jorgenson Avenue and north of Bridge Inn Road, which is experiencing rapid land use change from rural living to standard residential lots. The study area also includes: education uses with Plenty Valley Christian College and Yarrambat Primary School abutting the project boundary; recreation and open space at Werther Park, Doreen Recreation Reserve, Yarrambat Park and PGPR; and commercial premises interspersed along the alignment that are accessed directly via Yean Yan Road or via adjacent access roads.

The need to upgrade Yan Yean Road to a secondary arterial road to support the development of the North Growth Corridor was identified as early as 1990 within the *Plenty Valley Strategic Plan 1990*. Subsequent strategic plans provided for a 30 metre building setback and restricted access to facilitate its future upgrade. These early decisions shaped the land use and settlement pattern within Doreen, Yarrambat and Plenty and established the need for this Project.

9.2 RISK ASSESSMENT

Six initial risks for planning and land use were identified which had a risk rating of medium or above. These risks pertain to planning due diligence and land acquisition. For each initial risk, standard mitigation measures and EPRs such as a requirement for early and consistent consultation resulted in a lower residual risk.

9.3 IMPACT ASSESSMENT

Land use impact has been assessed for both the construction and operation of the Project. Land use impacts associated with the construction period are temporary and limited to the project boundary and can be managed through appropriate mitigation measures such as provision of alternative access and regular communication with road users and affected landowners.

The Project is generally consistent with and implements the objectives and strategies of Plan Melbourne, the WPS and the NPS. At the time of writing, the Project requires the partial or full acquisition of 96 parcels, which will result in permanent land use impact. Buildings will need to be removed from one parcel. For this parcel, it is a condition of the planning permit that the existing residential buildings be removed. Therefore, the removal of this dwelling is a requirement of the active planning permit rather than as a result of this Project. There is an existing additional occupied dwelling located on this parcel which will not be directly impacted by the Project and as such this parcel will remain in use for residential purposes.

In most cases, only partial acquisition of the land is required at a scale that would not impact on the existing land use, nor the availability, access and amenity of the land. Alternative access arrangements will be provided where access to Yan Yean Road is removed and, in some instances, existing access will be reinstated. It is considered that the Project will not impact on the orderly planning of an area as intended by existing strategic plans (identified within Section 5 and discussed in Section 7).

Land use impacts arising from the operation of the Project will be addressed through implementing standard and additional controls.

The impact assessment identified the following impacts for planning and land use:

- the Project is generally consistent with and implements strategic planning and local planning policy
- access will be provided via U-Turns, service lanes and reinstatement of driveway access
- in general, the Project will not impact on the future development potential of adjacent sites
- the Project will include opportunities to implement landscaping enhancement
- the Project will include opportunities to provide improved access to open spaces including PGPR and Yarrambat Park via SUP and footpaths.

With the implementation of the standard and additional controls detailed within Section 7, the construction and operational impacts to planning and land use are considered adequately managed.

9.4 PLANNING PATHWAY

The proposed planning pathway for the use and development associated with the Project is via a PSA to the NPS and WSP. The Amendment would amend the NPS and WSP to apply Clause 45.12 Specific Controls Overlay and Clause 45.01 Public Acquisition Overlay to affected land, and insert the *Yan Yean Road (Kurrak Road to Bridge Inn Road) (Stage 2) Upgrade Project Incorporated Document* into Schedules 45.12 and 72.04. This would exempt all use and development associated with the Project from the need for a planning permit, subject to conditions.

In accordance with the *Ministerial Guidelines for Assessment of Environmental Effects under the EE Act (Ministerial Guidelines) 2006*, the draft Amendment GC92 would be exhibited concurrently with the EES and a joint inquiry and Advisory Committee would be appointed to consider submissions on the draft PSA and EES. The Minister for Planning's assessment of the EES would inform the consideration of the draft PSA and the Commonwealth Minister for the Environment's approval decisions under the EPBC Act.

10 LIMITATIONS

This Report is provided by WSP Australia Pty Limited (*WSP*) for Major Road Projects Victoria (*Client*) in response to specific instructions from the Client and in accordance with WSP's proposal dated 24 October 2017 and agreement with the Client dated 14 October 2016 (Contract number 9383) (*Agreement*).

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APPENDIX A

PLANNING AND LAND USE RISK REGISTER



Major Road Projects Victoria

Project name: Yan Yean Road Stage 2 EIS Environmental Risk Register - Draft Post Environmental Risk Workshop & Specialist Input

Version: 5

Date: 26/06/2020

Author: Sabrina Chapman

Contributors: Matt Wright, Tim Harvey, Paige Pygler, Jude D'Amore, Will Parker, Clinton Rema, Arvid Delavaginnak, Louise McIntyre, Sarah Sawyer, Sam Verucci, Cameron Ryker, Rachel Mero, Samantha Fidge, Ben Buswell, George Busley, Melanne Steenrup

Review: Matt Peel



Technical Report H – Planning and land use impact assessment

No	Aspect	Impact Pathway	Activity	Initial Risk			Mitigation measures to inform Environmental Performance Requirement	EPR	Residual Risk			Rationale	Rule and Impact Assessment
				Likelihood	Consequence	Inherent Risk			Likelihood	Consequence	Residual Risk		
11	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	Site establishment	Almost Certain	Major	High	<ul style="list-style-type: none"> The Project must be designed and constructed to: <ul style="list-style-type: none"> Minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the environment and surrounding land uses. Other sensitive land uses such as educational facilities Recreational and community facilities Commercial and industrial sites. Consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable. Consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR L11	Possible	Major	Significant	<p>YSP has completed a Planning and Land Use Impact Assessment in November 2019. According to this assessment, the Project is located in predominantly low density residential and rural living areas within the metropolitan Green Wedge. The northern western end of the Project is within the Whittlesea Growth Corridor which is experiencing rapid land use change from rural living to residential.</p> <p>The Project's impacts on existing land uses during construction have been identified as follows:</p> <ul style="list-style-type: none"> Using Yan Yean Road for a purpose that a movement corridor (i.e. temporary construction and construction traffic). Potential relocation of existing utility services. Annuity impacts including disruption to access, air quality, noise, visual and an increase in construction traffic. 	Technical Report H – Planning and land use impact assessment
31	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	Earthworks	Unlikely	Major	High	<ul style="list-style-type: none"> The Project must be designed and constructed to: <ul style="list-style-type: none"> Minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the environment and surrounding land uses. Other sensitive land uses such as educational facilities Recreational and community facilities Commercial and industrial sites. Consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable. Consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR L11	Possible	Major	Significant	<p>YSP has completed a Planning and Land Use Impact Assessment in November 2019. According to this assessment, the Project is located in predominantly low density residential and rural living areas within the metropolitan Green Wedge. The northern western end of the Project is within the Whittlesea Growth Corridor which is experiencing rapid land use change from rural living to residential.</p> <p>The Project's impacts on existing land uses during construction have been identified as follows:</p> <ul style="list-style-type: none"> Using Yan Yean Road for a purpose that a movement corridor (i.e. temporary construction and construction traffic). Potential relocation of existing utility services. Annuity impacts including disruption to access, air quality, noise, visual and an increase in construction traffic. 	Technical Report H – Planning and land use impact assessment
51	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	Civils and structures	Unlikely	Major	High	<ul style="list-style-type: none"> The Project must be designed and constructed to: <ul style="list-style-type: none"> Minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the environment and surrounding land uses. Other sensitive land uses such as educational facilities Recreational and community facilities Commercial and industrial sites. Consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable. Consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR L11	Possible	Major	Significant	<p>YSP has completed a Planning and Land Use Impact Assessment in November 2019. According to this assessment, the Project is located in predominantly low density residential and rural living areas within the metropolitan Green Wedge. The northern western end of the Project is within the Whittlesea Growth Corridor which is experiencing rapid land use change from rural living to residential.</p> <p>The Project's impacts on existing land uses during construction have been identified as follows:</p> <ul style="list-style-type: none"> Using Yan Yean Road for a purpose that a movement corridor (i.e. temporary construction and construction traffic). Potential relocation of existing utility services. Annuity impacts including disruption to access, air quality, noise, visual and an increase in construction traffic. 	Technical Report H – Planning and land use impact assessment
71	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	Reinforcement	Likely	Moderate	Significant	<ul style="list-style-type: none"> The Project must be designed and constructed to: <ul style="list-style-type: none"> Minimise the design footprint and avoid, to the extent practicable, any temporary and permanent impacts on the environment and surrounding land uses. Other sensitive land uses such as educational facilities Recreational and community facilities Commercial and industrial sites. Consolidate or minimise the fragmentation of, and provide access to, residual land parcels to support future viable land use to the extent practicable. Consultation must occur with land managers and/or authorities responsible for the implementation of the relevant strategic land use plans and policies, including City of Whittlesea, Shire of Nillumbik, Melbourne Water and Yarra Valley Water. 	EPR L11	Possible	Moderate	Medium	<p>YSP has completed a Planning and Land Use Impact Assessment in November 2019. According to this assessment, the Project is located in predominantly low density residential and rural living areas within the metropolitan Green Wedge. The northern western end of the Project is within the Whittlesea Growth Corridor which is experiencing rapid land use change from rural living to residential.</p> <p>The Project's impacts on existing land uses during construction have been identified as follows:</p> <ul style="list-style-type: none"> Using Yan Yean Road for a purpose that a movement corridor (i.e. temporary construction and construction traffic). Potential relocation of existing utility services. Annuity impacts including disruption to access, air quality, noise, visual and an increase in construction traffic. 	Technical Report H – Planning and land use impact assessment
91	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	Operations	Possible	Minor	Medium	<ul style="list-style-type: none"> Mitigation measures have been applied during the design and construction phases of the Project and use of the existing road has been maintained to the extent practicable. As such, the residual risk of impact on land use during operation of the road is considered to be low. Where permanent land acquisition is unavoidable: Early and consistent consultation with affected land owners and tenants must occur Compensation for interests in acquired land must be assessed in accordance with Land Acquisition and Compensation Act 1986. 	EPR L12	Unlikely	Minor	Low	<p>YSP Planning and Land Use Impact Assessment (2019) states the potential impacts of the Project on land use during operation of the road are as follows:</p> <ul style="list-style-type: none"> Acquisition of land results in a permanent loss of revenue and closure of commercial premises Acquisition of land results in residential properties no longer being viable and useable for existing purposes <p>Resolution of compensation claims can be protracted.</p>	Technical Report H – Planning and land use impact assessment
111	Land use planning	Potential changes inconsistent with current or proposed future land use, including land acquisition, severance and occupation	Maintenance	Possible	Minor	Medium	<ul style="list-style-type: none"> Mitigation measures have been applied during the design and construction phases of the Project and use of the existing road has been maintained to the extent practicable. As such, the residual risk of impact on land use during operation of the road is considered to be low. Where permanent land acquisition is unavoidable: Early and consistent consultation with affected land owners and tenants must occur Compensation for interests in acquired land must be assessed in accordance with Land Acquisition and Compensation Act 1986. 	EPR L12	Unlikely	Minor	Low	<p>YSP Planning and Land Use Impact Assessment (2019) states the potential impacts of the Project on land use during operation of the road are as follows:</p> <ul style="list-style-type: none"> Acquisition of land results in a permanent loss of revenue and closure of commercial premises Acquisition of land results in residential properties no longer being viable and useable for existing purposes <p>Resolution of compensation claims can be protracted.</p>	Technical Report H – Planning and land use impact assessment

APPENDIX B

PLANNING POLICY FRAMEWORK ASSESSMENT



B1 PLANNING POLICY FRAMEWORK ASSESSMENT

PLANNING POLICY FRAMEWORK CLAUSE	PURPOSE	ASSESSMENT
Clause 11: Settlement	Clause 11 seeks to promote sustainable growth and development (11.01-1S Settlement), link activity centres by transport (11.01-1R Settlement - Metropolitan Melbourne, 11.03-1S Activity Centres and 11.03-1R Activity Centres - Metropolitan Melbourne), facilitate orderly development and the timely delivery of infrastructure (11.02-2S Structure Planning), and locate urban growth close to transport corridor and services (11.03-2S Growth areas).	<p>Victoria's unprecedented population growth has required the rapid intensification of urban areas in middle and outer Melbourne. Over the past decade one-million new people have moved to Melbourne, with approximately half moving into the city's outer municipalities: west (Wyndham, Melton); north (Hume, Whittlesea); and south-east (Cardinia and Casey). The population in the northern metropolitan sub-region is estimated to reach approximately 400,000-470,000 by 2031. The growth in population and the intensification of urban areas has resulted in increased travel demand, travel times, and accident hot spots.</p> <p>Whilst this Clause primarily relates to the development of land uses, the project supports Clause 11.01-1R, 11.03-1S and 11.03-1R by addressing current gaps in transport infrastructure within the outer suburbs and urban growth areas, and thereby enabling the efficient movement of goods and people to their destinations (Clause 11.02-2S, 11.03-2S).</p> <p>The project will ensure that there is the appropriate transport infrastructure to support existing and planned growth of residential, employment and supporting land uses in existing and urban and green wedge areas including Yarrambat and Doreen (Clause 11.01-1S).</p>

PLANNING POLICY FRAMEWORK CLAUSE	PURPOSE	ASSESSMENT
<p>Clause 12: Environmental and Landscape Values</p>	<p>Clause 12 seeks to protect and conserve Victoria's biodiversity (12.01-1S Protection of Biodiversity), ensure that there is no net loss to biodiversity as a result of the removal, destruction or lopping of native vegetation (12.01-2S Native Vegetation Management) and to protect and enhance river corridors, waterways, lakes and wetlands (12.03-1S River Corridors, Waterways, Lakes and Wetlands).</p>	<p>The project has sought to avoid and minimise native vegetation removal and habitat impacts to as far as practicably possible. Where impacts are unavoidable offsets will be secured in accordance with the relevant State legislation. Direction to retain native vegetation is to be provided to contractors in the form of maps identifying 'Areas of sensitive vegetation' (ASV) (Clause 12.01-1S). A detailed assessment is provided within Technical Report B – Flora and Fauna Impact Assessment.</p> <p>Melbourne Water's 2018 Healthy Waterways Strategy locates the project in the Yarra catchment and the Plenty River Lower Sub-catchment. The project does not intersect with any named waterways, although a number of tributaries of the Plenty River either traverse the road alignment or are in close proximity.</p> <p>Stormwater runoff will be managed using Water Sensitive Road Design (WSRD) elements to maintain the integrity of the waterways (Clause 12.03-1S). Consultation is currently being undertaken with Melbourne Water, Whittlesea City Council and Nillumbik Shire Council to understand if any duplication works have the potential to impact nearby drainage or flood paths. A detailed assessment is provided within Technical Report L – Surface Water Impact Assessment.</p>

PLANNING POLICY FRAMEWORK CLAUSE	PURPOSE	ASSESSMENT
Clause 13: Environmental Risks and Amenities	Clause 13 seeks to strengthen the resilience of communities by seeking to ensure that settlements and land uses appropriately respond to risks and environmental constraints, including flooding (13.03-1S Floodplain Management), land contamination (13.04-1S Contaminated and Potentially Contaminated Land), as well ensuring amenity is not prejudiced by noise and air quality emissions (13.05-1S Noise Abatement, 13.06-1S Air Quality Management).	<p>Consultation is currently being undertaken with Melbourne Water, Whittlesea City Council and Nillumbik Shire Council to understand if any duplication works have the potential to impact nearby drainage or flood paths (Clause 13.03-1S). A detailed assessment is provided within Technical Report L – Surface Water Impact Assessment.</p> <p>The Environmental Site Assessment undertaken to understand the potential contaminated land management risks for the confirmed the potential risk for contaminated land to adversely impact future land uses is low (Clause 13.04-1S). A detailed assessment is provided within Technical Report K – Contaminated Land Impact Assessment.</p> <p>The project does not trigger the requirement for noise attenuation based on the <i>VicRoads Traffic Noise Reduction Policy, 2005</i> (Clause 13.05-1S) (it is noted that this policy is currently under review by the Department of Transport (DoT) and may be subject to change). The majority of the most exposed residential locations along Yan Yean Road are predicted to have an increase of no more than 1 dBA in noise levels as a result of the project. A smaller number of properties are predicted to have an increase between 1 dBA and 3 dBA. As a generalisation, these noise level changes could be characterised as ‘barely perceptible’. A detailed assessment is provided within Technical Report I – Noise and Vibration Impact Assessment.</p> <p>Air quality impacts for the project will be managed in accordance with the Intervention Levels prescribed in the State Environment Protection Policy (Air Quality Management) [SEPP(AQM)] for NO2, PM10 and PM2.5 using the Air Quality Screening Tool (AQST) [Revision 3, October 2018] (Clause 13.05-1). A detailed assessment is provided within Technical Report M – Air Quality Impact Assessment.</p>

PLANNING POLICY FRAMEWORK CLAUSE	PURPOSE	ASSESSMENT
Clause 14: Natural Resource Management	Clause 14 seeks to protect the State's agricultural base by preserving productive farmland (14.01-1S Protection of agricultural land, 14.01-R Protection of agricultural land – Metropolitan Melbourne), to assist in protection and restoration of catchments, water bodies, ground water and the marine environment (14.02-1S Catchment Planning and Management) and to protect water quality (14.02-2S Water Quality).	Planning of the project has considered the State Environment Protection Policy (Waters), Construction Techniques for Sediment Pollution Control 1991 and the Environmental Guidelines for Major Construction Sites 1996. In accordance with these policies, consultation is currently being undertaken with Melbourne Water, Whittlesea City Council and Nillumbik Shire Council to understand if any duplication works have the potential to impact nearby land uses, drainage or flood paths (Clause 14.01-1 and 14.02-2). An assessment is provided within Technical Report L – Surface Water Impact Assessment.
Clause 15: Built Environment and Heritage	Clause 15 seeks to protect and enhance Melbourne's urban environments, including along transport corridors by embodying the principles of place making, managing road space, and the creation of boulevards in existing urban areas and growth areas (15.01-1S Urban Design, 15.01-1R Urban design - Metropolitan Melbourne); create healthy and sustainable neighbourhoods that encourage walking and cycling (15.01-4S Healthy Neighbourhoods, 15.01-4R Healthy Neighbourhoods - Metropolitan Melbourne, 15.02-1S Energy and Resource Efficiency); conserve areas of cultural heritage significance (15.03-1S Heritage Conservation, 15.03-2S Aboriginal Cultural Heritage).	Increased travel demand and traffic congestion impact on travel times and, as a consequence, liveability, as well as access to employment, goods and services. It also impacts on time available to pursue healthy lifestyles. The project would reduce travel times and contribute towards improved liveability outcomes (15.01-4S and 15.01-4R). The project achieves this by supporting alternative modes of transport, including public transport, walking and cycling (15.01-1S, 15.01-1R). The project has identified areas of environmental and cultural heritage significance (15.03-1S, 15.03-2S) and has sought to avoid impacts where possible, and otherwise minimise impacts to the extent practicable. Appropriate management and mitigation measures will be implemented via the project's Construction Environmental Management Plan. To the extent practical, the project has also sought to minimise impacts to existing vegetation in road reserves and private properties. Landscape strategies are currently being considered following the completion of roadworks and a mandatory CHMP (15169) has been prepared for the project and will need to be approved prior to construction.

PLANNING POLICY FRAMEWORK CLAUSE	PURPOSE	ASSESSMENT
Clause 18: Transport	<p>Clause 18 seeks to integrate land use and transport planning (18.01-1S Land Use and Transport Planning), the transport system (18.01-2S Transport system), sustainable personal transport within inner and outer Melbourne (18.02-1S Sustainable Personal Transport, 18.02-1R Sustainable Personal Transport - Metropolitan Melbourne), principal public transport network (18.02-2S Public Transport, 18.02-2R Principal Public Transport Network), ensuring an adequate supply of car parking that is appropriately designed and located (Clause 18.02-4S Car Parking), and developing the road system to include freight links in inner and outer Melbourne (18.02-3S Road System, 18.05-1S Freight Links, 18.05-1R Freight Links – Metropolitan Melbourne).</p>	<p>The project seeks to upgrade the existing road network. This upgrade aims to better integrate multiple modes of transport that use arterial roads – freight, cars, public transport, walking and cycling – and to deliver a transport system that provides a realistic choice in mode, elevates sustainable transport modes, and ensures equitable access to social and economic opportunities. It will further enable healthy lifestyles through the construction of improved connections to the existing walking and cycling paths and public transport (18.02-1). This will support the delivery of improved public transport services, better connect communities together, and support employment and access to goods and services (18.01-1S).</p> <p>The project will support the rapid growth of Mernda/Doreen in the North Growth Corridor and improve regional arterial road network capacity and connectivity to the M80, La Trobe NEIC, Melbourne Airport and the Northern Industrial Precinct. The project will contribute to an improved outer suburbs arterial road network, leading to greater efficiencies, improved productivity, and greater access to jobs, services and markets.</p>

PLANNING POLICY FRAMEWORK CLAUSE	PURPOSE	ASSESSMENT
<p>Clause 19: Infrastructure</p>	<p>Clause 19 seeks to facilitate the timely provision of planned infrastructure to communities through the collection and implementation of development contributions (19.03-1S Development and Infrastructure Contributions Plans).</p>	<p>The project is generally consistent with strategic planning for the area. Future growth will be concentrated in existing urban areas in the south and the developing areas of Mernda-Doreen in the north. The lack of north-south road capacity and connectivity poses a constraint to future growth, accessibility and public transport services. Yan Yean Road is an arterial route servicing this corridor and infrastructure upgrades have not kept pace with urban development.</p> <p>The project is partially affected by two Development Contributions Plan Overlays (DCPO5 and DCPO6). The Overlays set out a summary of the costs and contributions prescribed in the <i>Mernda Strategy Plan Development Contributions Plan</i>. This includes provision for the collection of development contributions towards new road infrastructure and intersection treatments towards the intersection of Bridge Inn Road and Yan Yean Road. The project requires the acquisition of land within DCPO5 and DCPO6.</p> <p>Confirmation is being sought from Council and MRPV on the implications of this acquisition on the land budget. Development contributions are not required for this project (Clause 19.03).</p>

APPENDIX C

LOCAL PLANNING POLICY FRAMEWORK



Table C.1 Whittlesea Planning Scheme – MSS and LPPF

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
Municipal Strategic Statement		
Clause 21.02 Municipal Profile	<p>The WPS states <i>“the City of Whittlesea is third fastest and largest growing municipality in Victoria and the sixth largest in Australia. The population is expected to reach 300,000 by 2030 with growth concentrated into the developing areas of Mernda-Doreen, South Morang, Epping North, Wollert and Donnybrook/Woodstock”</i>.</p> <p>Clause 21.02 states <i>“north-south road capacity is currently severely limited beyond the City”</i> and <i>“public transport services in existing growth areas of Epping North and Mernda-Doreen have not kept pace with urban development. The lack of north-south road capacity and continuous east west movements in the established parts of the municipality pose a significant constraint on future growth and development of the City. These movements are important for private car based travel, freight movements and public transport routes”</i>.</p>	<p>The City of Whittlesea has identified that connectivity poses a significant constraint on future growth. Transport is identified as one of eight key issues facing Whittlesea. The lack of north south road capacity and continuous east-west routes in established parts of the municipality are identified as a significant constraint.</p> <p>Traffic data indicates that Yan Yean Road carries in excess of 20,000 vehicles per day. Flows during peak times are heavy, with the road operating close to its theoretical capacity. The upgrade is necessary to accommodate the increased travel demand generated from the existing and planned growth areas of Mernda and Doreen and improve connectivity to regionally significant employment nodes and services.</p>
Clause 21.03 Council Vision and Strategic Framework	<p>Clause 21.03-1 Community plan identifies the major land use planning objectives and key strategic directions of the <i>‘Shaping our Future’</i> Community Plan 2030 including <i>“accessibility in, out and around our City”</i>.</p>	<p>The project responds to Council’s vision of a road network that meets the community’s needs (Clause 21.03). The project will assist in the improvement of the capacity of the arterial road network required to support the anticipated growth of Doreen, as well as provide for more efficient public transport services, improved freight movement, and critical areas of employment as designated within <i>Plan Melbourne</i>.</p>

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
<p>Clause 21.04 Settlement</p>	<p>Clause 21.04-2 Urban Growth goes further to state “<i>The construction of roads and the provision of mixed use employment generating centres are of key importance</i>”.</p> <p>Clause 21.04-2 seeks to ensure that rapid urban growth requires continued careful management to ensure that environmental assets are not lost, non-urban areas are not threatened by uncontrolled or ill-defined limits to urban expansion, longer term strategic approaches to the allocation of land uses are not compromised by incremental decision making.</p> <p>Clause 21.04-4 Open Space Network states the City of Whittlesea contains several significant parks such as PGPR and Yan Yean Reservoir. Strategy 1.7 seeks to “<i>provide growth areas with adequate, good quality open space as they develop</i>”.</p>	<p>Yan Yean Road provides connectivity to major employment clusters and links residents to the suburbs of Mernda, South Morang, Diamond Creek and Greensborough. However, Yan Yean Road is currently at or exceeding capacity and journey times are increasing and becoming more unreliable. Future population growth in the City of Whittlesea will place increased demand on the existing transport infrastructure due to increased congestion.</p> <p>The project supports Clause 21.04 by providing additional transport infrastructure that improves network efficiency and connects people to jobs and services, supporting existing and future growth of residential, employment and educational facilities in the region.</p>
<p>Clause 21.08 Built Environment and Heritage</p>	<p>Clause 21.08 seeks to protect and enhance Whittlesea’s urban environments by supporting places and spaces that connect people, and to progressively upgrade the image and appearance of the City by focussing on the retention of local environmental, historic and cultural features, as well as landscape qualities and urban design improvements.</p>	<p>The project has identified areas of environmental and cultural heritage significance and has sought to avoid impacts where possible, and otherwise minimise impacts to the extent practicable. Appropriate management and mitigation measures will be implemented via the project’s Environmental Management Framework. A mandatory Cultural Heritage Management Plan (CHMP) (15169) has been prepared for the project.</p> <p>There are two River Red Gum trees located on the northeast corner of the Yan Yean and Doctors Gully Road intersection that are protected by a Heritage Overlay (HO191). This heritage place is within the project area and will be impacted by the project. A planning permit for works within the heritage overlay will be required. Technical Report M considers the social and cultural values of River Red Gums and the Technical Report C – Biodiversity addresses the removal and offsets required for the trees.</p>

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
Clause 21.11 Transport	<p>A key focus for WCC is the development of an integrated transport system and a key objective of Clause 21.11-1 is <i>“to establish an efficient, interconnected multi-modal transportation system which increases the level of accessibility and choice within and beyond the City of Whittlesea”</i>. Roads are part of this integrated transport framework and it is acknowledged that existing and future activity centres should be serviced by high capacity rail and have good access to the arterial road network. A key strategy (1.2) of Clause 21.11-1 Integrated Transport is <i>“to provide new and improved arterial roads to enable the provision of trunk public transport services between activity areas, railway stations and public transport interchanges”</i>. Recognising the need to create more self-contained communities within the outer growth areas with less reliance on established areas for services and employment, Strategy 1.12 seeks to <i>“improve the level of accessibility to services and facilities within growth areas so that individual communities are more self-sufficient and do not rely on access to existing services and facilities in existing communities”</i>. WCC seeks to ensure that development integrates and improves access to walking, cycling and public transport networks that link with key destinations such as schools, activity centres, community facilities and public transport interchanges.</p>	<p>The project will make a significant contribution to the improvement of the capacity of the arterial road network which is essential to provide existing and planned communities with better access to services and critical areas of employment. In particular, Yan Yean Road links residents to the suburbs of Mernda, South Morang, Diamond Creek and Greensborough which provide regional employment and services and access to the metropolitan rail network.</p> <p>The project will support the rapid growth of Mernda/Doreen in the North Growth Corridor and improve regional arterial road network capacity and connectivity to critical employment clusters in the broader North Growth Corridor including Thomastown, Somerton and the La Trobe National Employment and Innovation Cluster.</p> <p>The project will also help to improve connectivity and safety for pedestrians and cyclists through the provision of a new, separated shared use path on the western side, and a footpath on the eastern side of Yan Yean Road. These paths will provide pedestrian and cyclist links from Diamond Creek to Doreen, with future connections to Bridge Inn Road, Mernda Railway Station, Yarrambat Park and Plenty Gorge Parkland Reserve.</p>

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
Local Planning Policies		
Clause 22.03 Bushfire Management Policy	<p>The <i>Regional Bushfire Planning Assessment 2012</i> outlines that bushfire hazard is high along the Plenty River adjacent to established and emerging residential estates and within residential areas that are bordered by non-urban edges. Objective 2 of Clause 21.07-4 is to <i>reduce the impact of bushfire risk</i>. Clause 22.03 Bushfire Management Policy applies to Yan Yean Road as it is on the western edge of a designated Bushfire Prone Area. Clause 22.03-3 requires that in considering any PSA it is policy to ensure that the views of the Country Fire Authority and relevant publications (as outlined in Clause 13.05) are taken into account in decision making.</p>	<p>The project is located within a Bushfire Prone Area. However, the project will not intensify local wildfire risk and may improve access and movement in the region in line with Clause 22.03.</p>
Clause 22.04 Heritage Conservation Policy	<p>This policy applies to all land within a Heritage Overlay, and all dry stone walls affected by Clause 52.37 within the City of Whittlesea. Should dry stone walls be identified during construction a dry-stone wall management plan will be required to be prepared.</p> <p>A key objective is “<i>To identify, protect and maintain the integrity and character of Whittlesea’s heritage places</i>”.</p> <p>Clause 22.04 seeks to identify, protect and maintain the integrity and character of Whittlesea’s heritage places, and discourage changes to the shape and position of roads and footpaths if these are contributory elements of a heritage place or precinct.</p>	<p>This policy applies to all land within a Heritage Overlay and all dry-stone walls affected by Clause 52.33 within the City of Whittlesea.</p> <p>The project has sought to avoid impacts where possible, and otherwise minimise impacts to the extent practicable and feasible. Should dry stone walls be identified during construction a dry-stone wall management plan will be required to be prepared.</p> <p>There are two River Red Gum trees located on the northeast corner of the Yan Yean and Doctors Gully Road intersection that are protected by a Heritage Overlay (HO191). This heritage place is within the project area and will be impacted by the project. A planning permit for works within the heritage overlay will be required.</p>

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
<p>Clause 22.10 River Red Gum Protection Policy</p>	<p>Clause 22.10 of the LPPF outlines the importance of River Red Gum trees within the municipality.</p> <p>Clause 22.10 states that “<i>Existing and future urban areas such as Mill Park, South Morang, Mernda/Doreen and Wollert contain significant River Red Gum trees and associated habitat.</i>”</p> <p>Clause 22.10 seeks to ensure that development of existing and future urban and rural areas takes into account the presence of and plans for the retention, enhancement and long term viability of River Red Gum trees.</p>	<p>An assessment of River Red Gums affected by the proposed upgrade has been undertaken to identify, minimise and mitigate potential impacts to vegetation. Following completion of a final design, a Tree Protection Management Plan for River Gums, including consideration of this policy, will be prepared detailing how trees proposed for retention will be protected. Following avoid and minimise efforts it is anticipated that two River Red Gums covered by HO191 will be removed and offset.</p>

Table C.2 Nillumbik Planning Scheme – MSS and LPPF

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
Municipal Strategic Statement		
Clause 21.03 Municipal Profile and Key Influences	The Plenty/Yarrambat Corridor area has ongoing pressure for low density residential development and has further growth potential. Its proximity to central Melbourne, the Whittlesea Growth Corridor and improved road access provided by the Western Ring Road places increased pressure on this area from development. With respect to infrastructure development issues, key influences are <i>“the high level of car usage and projected population increase may encourage the upgrade of arterial roads in the municipality. The construction of new roads or road upgrades should be considered of potential impacts on the rural amenity and should be consistent with local environmental values”</i> .	Rural land consists of rural residences set in a natural environment interspersed with small scale agricultural farms. Much of this rural area is highly valued by residents and visitors for its strategic environmental and landscape qualities. Sites of Environmental Significance within vicinity of the project include the PGPR and Plenty River and in excess of 100 km of significant roadside vegetation. Rural regional and district open space areas include Yarrambat Park which provides a public golf course, driving range, archery club, aero club, pistol club and trotting facilities.
Clause 21.04 Vision – Strategic Framework	The framework plan under Clause 21.04-4 identifies a low density residential corridor at Plenty and Yarrambat and the need for a <i>“road network which joins the centres and allows access to and from non-urban and urban areas”</i> .	This project will complete the duplication of this important north-south connection and improve public and active transport links to the future Mernda Town Centre and existing station.

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
<p>Clause 21.05 Settlement and Housing</p>	<p>Clause 21.05-1 of the LPPF provides objectives for housing and settlement patterns within the Shire of Nillumbik to maintain sustainability as a 'green wedge' municipality.</p> <p>A key objective of this Clause is to cater for the projected increase in households through the provision of a range of housing types within the municipality that reflect the neighbourhood character, and protect natural and cultural heritage values. In particular, this Clause seeks to “<i>focus new residential subdivision and development in areas that are zoned residential and are currently vacant in Greensborough, Diamond Creek and Plenty, and new low density subdivision and residential development in areas zoned for that purpose in Yarrambat and Plenty and other appropriately zoned areas.</i>”</p> <p>Another key objective is “<i>to contain urban development and maintain no-urban breaks between existing urban areas and townships, and between townships.</i>”</p>	<p>Yan Yean Road is the primary north south arterial road providing arterial road access between Doreen, Yarrambat and Plenty. Due to unprecedented growth within Doreen, there is increasing pressure on the transport network within the region to support travel demand. This project will support the settlement strategy within Nillumbik Shire to encourage development into appropriately zoned areas including Yarrambat and Plenty and provide these residents with improved arterial road network connectivity.</p>
<p>Clause 21.05 Rural Land Use</p>	<p>Key objectives of Clause 21.05-2 are “<i>to limit the fragmentation of land in rural areas</i>” and “<i>to protect and enhance rural landscape character</i>”.</p> <p>Strategies to achieve these objectives include encouraging new development within rural residential zones in Yarrambat and Plenty, and that building and works in these areas maintain landscape qualities.</p> <p>Clause 21.05-2 identifies that the Nillumbik Shire Council Roadside Management Plan should be consulted in relation to works on roadsides.</p>	<p>The project is located in a predominantly low density residential and rural living area, and is adjacent to the rural conservation zone. The north-western extent of the project is within the Whittlesea Growth Corridor, which is experiencing rapid land use change from rural living to residential.</p> <p>Yan Yean Road connects Yarrambat and Plenty, which have been identified in Clause 21.05-2 as areas where new development is to be encouraged. The project will improve the network connectivity to these development regions, while seeking to maintain existing environmental and amenity values.</p>

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
<p>Clause 21.05-3 Environment, Conservation Landscape</p>	<p>Clause 21.05-3 seeks “to protect and enhance sites of environmental significance” within the municipality through the management of threatening processes that cause environmental degradation.</p> <p>Measures to achieve this include restricting development and land uses that may impact native flora and fauna, implementing the Nillumbik Shire Council Roadside Management Plan 2012 and using indigenous vegetation in roadside reservations.</p>	<p>These two clauses and their objectives and strategies are linked with the local Roadside Management Policy. Major environmental features within proximity of Yan Yean Road are Plenty River and roadside vegetation. The project will enhance vehicle and active transport links to the Plenty Gorge Parkland Reserve. Protection of roadside vegetation is discussed in further detail under Clause 22.06.</p>
<p>Clause 21.05 Infrastructure</p>	<p>Clause 21.05-5 outlines the importance of providing appropriate infrastructure services, including transport networks, with a key objective “to provide safe and efficient roads and road links within the municipality and to the wider region.”</p> <p>This Clause acknowledges the increase in traffic levels along Yan Yean Road resulting from residential development within the Whittlesea Growth Corridor, and seeks to “ensure that land use adjacent to Yan Yean Road is compatible with the long term arterial road transport function of the road and that direct access to properties from Yan Yean Road is minimised.”</p> <p>Clause 21.05-5 also identifies the need for Yan Yean Road to be duplicated from Diamond Creek Road to Kurrak Road, and encourages setbacks for development and use on properties with road frontage along this section of road.</p>	<p>Yan Yean Road is currently at, or very close to capacity, with no spare intersection capacity at Ironbark Road. The full duplication will support the increase travel demand to be generated from population growth within the Whittlesea Growth Corridor, Yarrambat and Plenty.</p> <p>Planning has long identified the need for Yan Yean Road to provide “an arterial road transport function” The project is entirely consistent with the objectives and strategies under Clause 21.05-5.</p>

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
Local Planning Policies		
Clause 22.05 Aboriginal Cultural Heritage Policy	Clause 22.05 provides objectives and policy which aims to preserve and protect places and items of Aboriginal cultural heritage importance and sensitivity. The objectives are to provide for the identification of the sites of Aboriginal cultural significance and the appropriate level of management in consultation with the local Aboriginal community, and to ensure new uses, developments and works do not impede on or detract from sites and features of Aboriginal cultural heritage and archaeological significance.	The project is located within areas of Aboriginal cultural heritage sensitivity. A mandatory CHMP (15169) has been prepared for the project and will need to be approved prior to construction.
Clause 22.06 Roadside Management Policy	Clause 22.06 Roadside Management Policy applies to all roadsides throughout the Shire and aims to maintain biodiversity and vegetation on roadsides. Roadsides are an integral part of maintaining biodiversity, and provide wildlife with corridors for movement between larger areas of habitat. The objective of this policy is “to maintain and enhance the environmental, historical and landscape values of the roadsides in the Shire”, through the implementation of the <i>Nilumbik Roadside Management Plan 2012 (NRMP)</i> , which is a reference document within the NPS.	Major environmental features within proximity of Yan Yean Road are Plenty River and roadside vegetation in the Shire. Where road upgrades and widening are proposed it is the policy of the NRMP Plan “to avoid and minimise environmental damage and negative impacts on vegetation, wildlife habitat and existing pathway infrastructure as a result of construction or maintenance works, while preserving a safe and efficient road system”. The NRMP provides design guidance and environmental management protocols. Where possible, vegetation has been avoided and a landscape strategy will be prepared to demonstrate that significant landscaping and revegetation will occur to replace the loss of vegetation along the corridor. Construction will be undertaken in accordance with the NRMP, where relevant.

CLAUSE	PURPOSE / OBJECTIVE	RELEVANCE TO THE PROJECT
Clause 22.12 Neighbourhood Character Policy	<p>Clause 22.12 applies to development and works within the General Residential, Residential Growth, Neighbourhood Residential, Low Density Residential and Township zones within the Nillumbik Planning Scheme. The Clause seeks to ensure development follows community and environmental values, and that development is “<i>responsive to the preferred future character of the area</i>”.</p> <p>Design Objectives and Design Responses are provided within Clause 22.12-3 to achieve the desired future character of the area.</p>	<p>This policy does not apply to Yarrambat which is affected by the Design and Development Overlay-Schedule 2 Yarrambat and Yarrambat Extension Area Design and Development Area (DDO2). DDO2 which seeks to ensure that adverse impacts of development upon the local drainage system are minimised and to protect locally indigenous vegetation, and to encourage further planting of locally indigenous vegetation.</p> <p>A landscape strategy will be prepared for the project and provide detail on the appropriate planting for this area.</p>
Clause 22.13 Wildfire Management Policy	<p>Clause 22.13 Wildfire Management Policy of the MSS applies to land identified within the Bushfire Prone Area maps. The relevant objective of this policy is “<i>to avoid intensifying local wildfire risk to people and property through inappropriately located, designed or managed uses or development</i>”.</p>	<p>The project will not increase the risk to life, property, community infrastructure and the natural environment from bushfire and the project may improve emergency access to bushfire prone areas. Therefore, the project does not directly address bushfire risk.</p>

APPENDIX D

ZONES AND OVERLAYS ASSESSMENT



D1 ZONES

ZONE	PURPOSE	PERMIT REQUIREMENT
City of Whittlesea		
Clause 32.04 Mixed Use Zone (MUZ2) Schedule 2 - Doreen Neighbourhood Activity Centre (South-West Corner of Yan Yean Road and Bridge Inn Road)	To provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality. In accordance with Clause 62.01 and Clause 32.04-9 a planning permit is not required for roadworks in this zone.	A planning permit is not required.
Clause 32.08 General Residential Zone (GRZ1) Schedule 1	To encourage development that respects the neighbourhood character of the area. To encourage a diversity of housing types and housing growth particularly in locations offering good access to services and transport. In accordance with Clause 62.01 and Clause 32.08-9 a planning permit is not required for roadworks in this zone.	A planning permit is not required.
Clause 36.04 Road Zone Category 1 (RDZ1) Category 2 (RDZ2)	To identify significant existing roads and identify land which has been acquired for a significant proposed road. In accordance with Clause 62.01 and Clause 62.02-2, a permit is not required for roadworks in these zones.	A planning permit is not required.
Nilumbik Shire		
Clause 32.03 Low Density Residential Zone (LWDRZ)	To provide for low-density residential development on lots which, in the absence of reticulated sewerage, can treat and retain all wastewater. In accordance with Clause 62.01 and Clause 32.03-4 a planning permit is not required.	A planning permit is not required.

ZONE	PURPOSE	PERMIT REQUIREMENT
Clause 35.04 Green Wedge Zone (GWZ)	To recognise, protect and conserve green wedge land for its agricultural, environmental, historic, landscape, recreational and tourism opportunities, and mineral and stone resources. In accordance with Clause 62.01 and Clause 62.02-2, a planning permit is not required for roadworks in the zone.	A planning permit is not required.
Clause 35.06 Rural Conservation Zone (RCZ3) Schedule 3 - Conservation Values	To protect and enhance the natural environment and natural processes for their historic, archaeological and scientific interest, landscape, faunal habitat and cultural value. In accordance with Clause 62.01 and Clause 62.02-2, a planning permit is not required for roadworks in the zone. In accordance with RCZ Schedule 3 a permit will be required if 'Earthworks which change the rate of flow or the discharge point of water across a property boundary'.	A planning permit will be required for Earthworks.
Clause 36.01 Public Use Zone PUZ1 - Service and Utility PUZ2 - Education	To recognise public land use for public utility and community services and facilities. To provide for associated uses that are consistent with the intent of the public land reservation or purpose. In accordance with Clause 62.01 and Clause 36. 01-2 a planning permit is not required. In accordance with Clause 62.01 and Clause 36. 01-2 a permit is not required for roadworks in the zone.	A planning permit is not required.
Clause 36.02 Public Park and Recreation Zone PPRZ	To recognise areas for public recreation and open space. To protect and conserve areas of significance where appropriate. To provide for commercial uses where appropriate. In accordance with Clause 62.01 and Clause 62.02-1 a permit is not required for roadworks within the zone.	A planning permit is not required.
Clause 37.01 Special Use Zone (SUZ3) Schedule 3 - Plenty Valley Christian College	To recognise or provide for the use and development of land for specific purposes as identified in a schedule to this zone. In accordance with SUZ Schedule 3 a planning permit will not be required.	A planning permit is not required.

D2 OVERLAYS

OVERLAY	PURPOSE	PERMIT REQUIREMENT
City of Whittlesea		
Clause 42.02 Vegetation Protection Overlay (VPO1) - Significant Vegetation (River Redgum Grassy Woodland)	To protect areas of significant vegetation and ensure development minimises the loss of vegetation. In accordance with VPO schedule 1 a permit is required to remove, destroy or lop native vegetation.	A planning permit is required to remove, destroy or lop native vegetation. The ecology assessment will confirm the extent of vegetation removal under this overlay.
Clause 43.03 Incorporated Plan Overlay (IPO1) - Mernda Strategy Plan	Schedule 1 incorporates the MSP into the WPS and seeks to provide a framework for environmental improvement, social progress and economic growth. The project is generally in line with the specified incorporated plans. A planning permit is not required under this Overlay.	A planning permit is not required.
Clause 43.04 Development Plan Overlay (DPO5) Schedule 5 (DPO5) - Mernda Development Plan	The DPO requires a development plan to be prepared. DPO Schedule 5 (DPO5) relates to Mernda Development Plan and requires that buildings and works comply with the plan. The project is in generally in accordance with the Mernda Development Plan. A planning permit is not required under this Overlay.	A planning permit is not required.
Clause 45.06 Development Contribution Plan Overlay (DCP05 & DCP06) Development Contributions Plans Overlay (DCP05) Development Contributions Plans Overlay (DCP06)	To identify areas which require the preparation of a development contributions plan for the levying contributions for the provision of works, services and facilities before development can commence. DCPO5 applies to the land within the Precinct 2A of the Mernda Strategy Plan. A planning permit is not required under this Overlay. DCPO6 applies to the land within the Precinct 2B of the Mernda Strategy Plan. A planning permit is not required under this Overlay.	A planning permit is not required.

OVERLAY	PURPOSE	PERMIT REQUIREMENT
Nillumbik Shire		
<p>Clause 42.01</p> <p>Environmental Significant Overlay (ESO1) - Sites of Faunal and Habitat Significance</p>	<p>To identify areas where the development of land may be affected by environmental constraints.</p> <p>In accordance with ESO Schedule 1 a planning permit is required to remove vegetation not included in Clause 42.01-3.</p>	<p>A Planning Permit is required to remove vegetation not specifically listed under Clause 42.01-3. The ecology assessment will confirm the extent of vegetation removal under this overlay.</p> <p>To avoid and minimise impacts to native vegetation and habitat a methodology was derived in collaboration between ecologists, arborists, designers and MRPV. Native vegetation and habitat within the Specific Controls Overlay (SCO) was prioritised for retention value as Low, Medium or High. This data was provided to the designers to prioritise efforts to avoid and minimise impacts on the higher quality vegetation and habitat during reference design development wherever possible, in order to meet local, State and Commonwealth environmental legislative requirements. From this process, No Go Zones were developed to designate areas to be retained or to be avoided, if possible, at the detailed design phase. Refer to the <i>No Go Zone Native Vegetation Avoidance and Mitigation Procedure (WSP, 2019)</i> for further detail on No Go Zones and fencing procedures.</p>

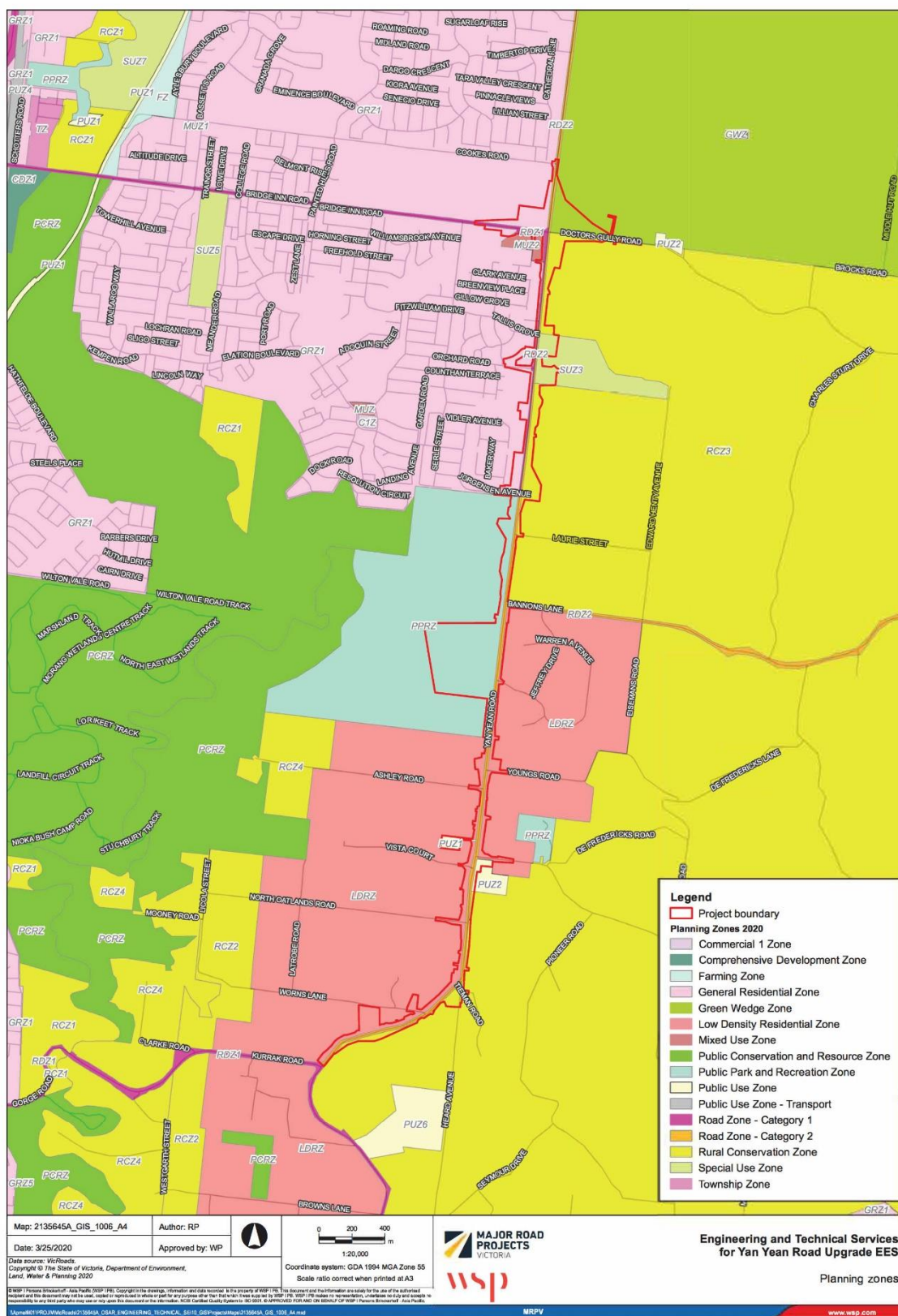
OVERLAY	PURPOSE	PERMIT REQUIREMENT
<p>Clause 43.01</p> <p>Heritage Overlay (HO191) - River Red Gums (2) 25 Doctors Gully Road, Doreen</p>	<p>To conserve and enhance heritage places of natural or cultural significance.</p> <p>In accordance with Clause 43.01-1 a permit is required to remove, destroy or lop a tree if the schedule to this overlay specifies the heritage place as one where tree controls apply.</p> <p>The project impacts on HO191. Tree controls apply and are listed on the NT and the trees are not listed on the VHR or VHI.</p>	<p>A planning permit is required.</p>
<p>Clause 43.02</p> <p>Design and Development Overlay (DDO2) - Yarrambat and Yarrambat Extension Area Design and Development Area</p>	<p>To identify areas which are affected by specific requirements relating to the design and built form of new development. Schedule 2 to the DDO relates to Yarrambat and Yarrambat Extension Area Design and Development Area.</p> <p>In accordance with DDO Schedule 2 a planning permit is not required under this Overlay.</p>	<p>A planning permit is not required.</p>
<p>Clause 44.06</p> <p>Bushfire Management Overlay (BMO)</p>	<p>To ensure that the development of land prioritises the protection of human life and strengthens community resilience to bushfire.</p> <p>In accordance with Clause 44.06-2 a planning permit is not required under this Overlay.</p>	<p>A planning permit is not required.</p>
<p>Clause 45.01</p> <p>Public Acquisition Overlay (PAO1) - Vic Roads – Road Widening</p>	<p>To identify land which is proposed to be acquired by a Minister, public authority or municipal council.</p> <p>In accordance with the schedule to Clause 45:01-1 a permit is not required where VicRoads (roads corporation) is the acquiring authority for the purpose of road widening. MRPV are acting under delegated authority of VicRoads a planning permit is therefore not required.</p>	<p>A planning permit is not required.</p>
<p>Clause 45.03</p> <p>Environmental Audit Overlay (EAO)</p>	<p>To ensure that potentially contaminated land is suitable for a use which could be significantly adversely affected by any contamination.</p> <p>In accordance with 45.03-1 a planning permit is not required under this Overlay.</p>	<p>A planning permit is not required.</p>

APPENDIX E

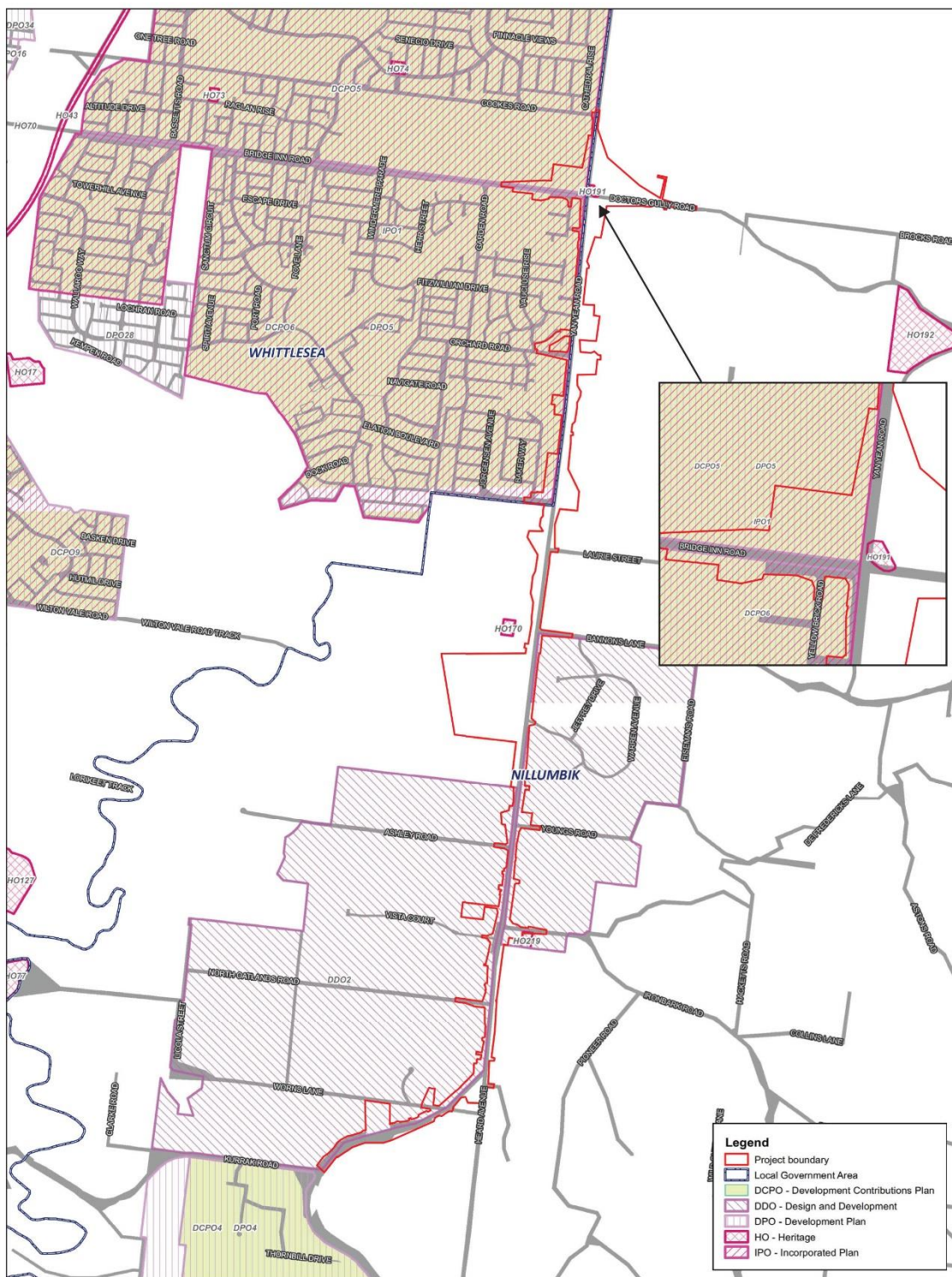
ZONES AND OVERLAYS MAPPING



APPENDIX E-1 ZONES



APPENDIX E-2 OVERLAYS



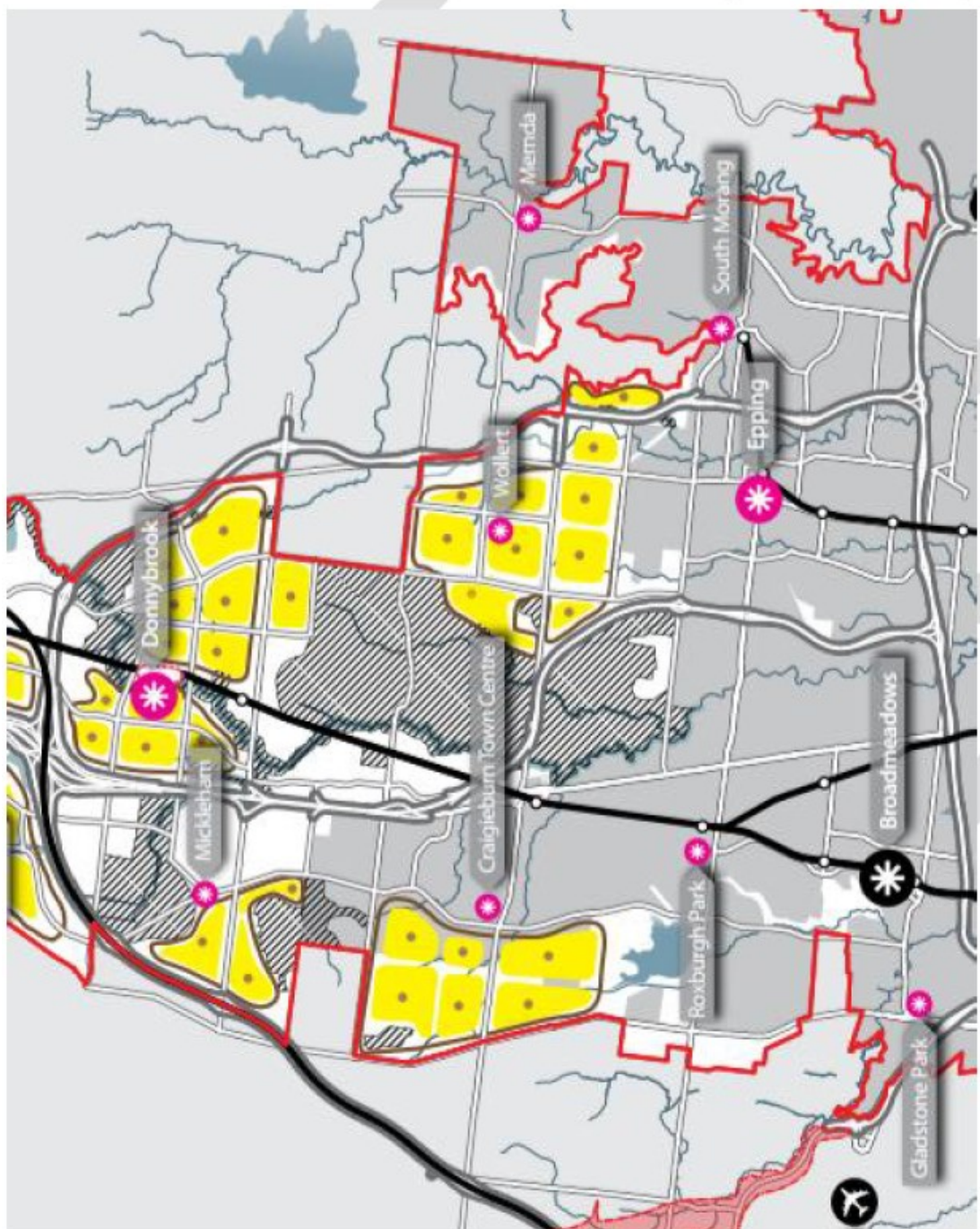
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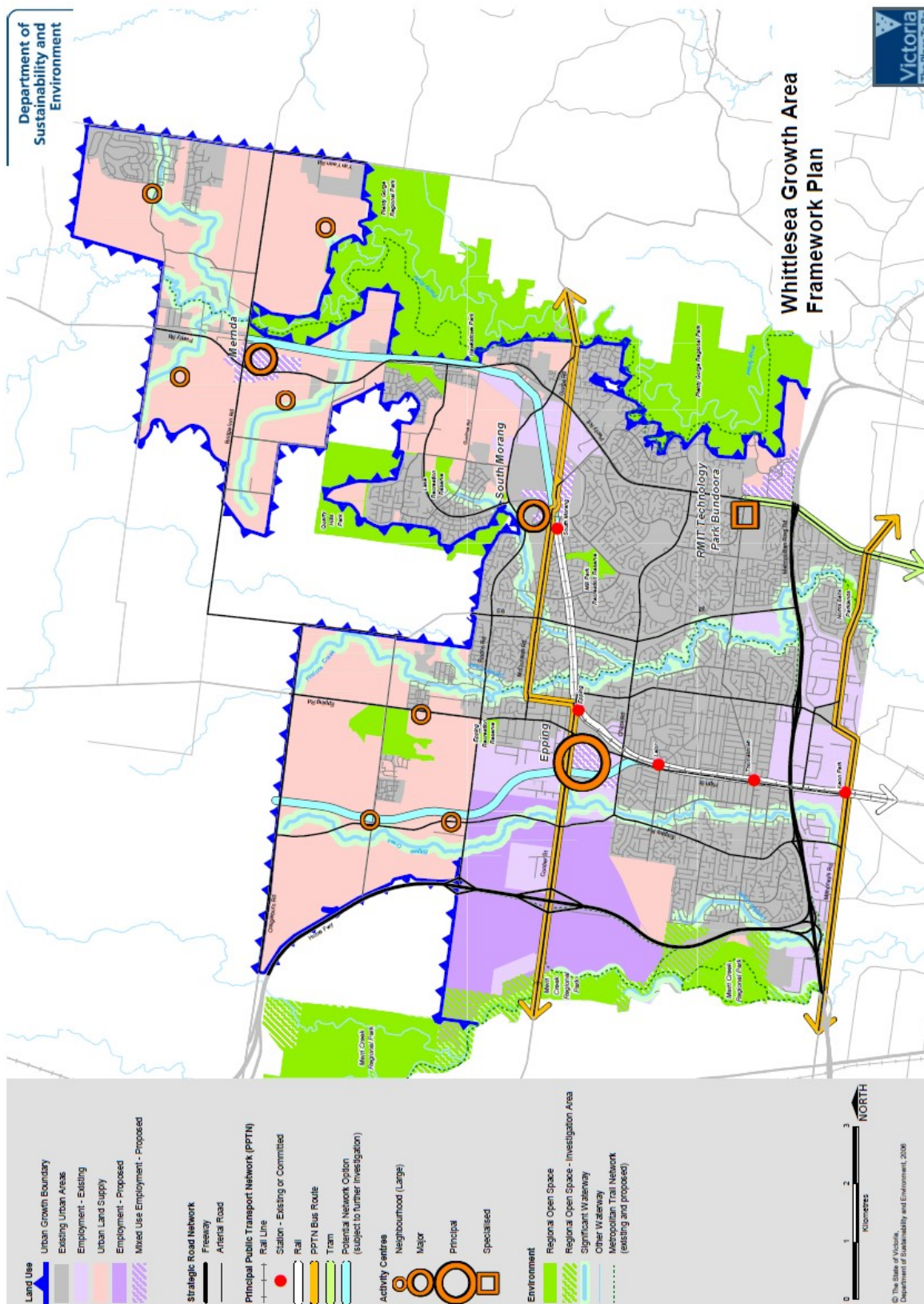
- Project boundary
- Local Government Area
- DCPO - Development Contributions Plan
- DDO - Design and Development
- DPO - Development Plan
- HO - Heritage
- IPO - Incorporated Plan

APPENDIX F

WHITTLESEA GROWTH AREA FRAMEWORK







APPENDIX G

CERTIFICATES OF TITLE



G1 CERTIFICATES OF TITLE

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\LP11297	635-643 YAN YEAN ROAD	VOLUME 08449 FOLIO 229	NO	NO	NO	NO		
1\LP34910	526-538 YAN YEAN ROAD	VOLUME 08144 FOLIO 200	NO	NO	NO	NO		
1\LP51225	30 DOCTORS GULLY ROAD	VOLUME 08371 FOLIO 568	NO	NO	NO	NO		
1\LP65401	870 YAN YEAN ROAD	VOLUME 10987 FOLIO 549	NO	NO	NO	NO		
1\LP74130	638-646 YAN YEAN ROAD	VOLUME 08656 FOLIO 516	NO	NO	SECTION 173	YES	Lot 1 074130	Planning Permit No. 441/2007/14P issued on 30 June 2008 by Nillumbik Shire Council. Condition 4 requires the owner to enter into a Section 173 Agreement. The agreement contains building and works within a building envelope for Lot 2, restricts mature vegetation clearance from the north boundary of lot 2 (northern boundary is perpendicular to Yan Yeans Road) and establishes on-site effluent and wastewater disposal, except with the prior written consent of Council. Plan No. LP74130 includes the creation of a road easement (R1) to set aside land for the future widening of Yan Yeans Road.
1\LP80247	780 YAN YEAN ROAD	VOLUME 08786 FOLIO 256	E-1 (Drainage)	NO	NO	NO		

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\LP83422	573 YAN YEAN ROAD	VOLUME 08780 FOLIO 751	NO	NO	NO	NO		
1\LP94920	830 YAN YEAN ROAD	VOLUME 08952 FOLIO 601	NO	NO	NO	NO		
1\PS305570	790A YAN YEAN ROAD	VOLUME 10037 FOLIO 364	E-2 (Electricity [Powerline] - S.E.C.V) & E-1 (Way & Water Supply - Plenty - Yarrambat W.W.T)	E-1 & E-2	NO	NO		
1\PS314043	25 DOCTORS GULLY ROAD	VOLUME 10139 FOLIO 301	E-1 (Electricity - S.E.C.V) E-4 (Electricity - S.E.C.V) E-5 (Drainage) E-2 (Electricity - S.E.C.V) E-3 (Electricity - S.E.C.V)	E-5 (Drainage) E-2 (Electricity - S.E.C.V) E-3 (Electricity - S.E.C.V)	SECTION 173	YES	Lot 1 PS43G	Planning Permit No. 566/2001/02P issued on 20/6/02 and amended 8/8/02 by Nillumbik Shire Council for the use and development of caretaker residence and agricultural use (racehorse breeding and training complex). Condition 2 requires the owner to enter into a Section 173 Agreement. Of relevance, the agreement states that the land shall not be subdivided. Land was acquired by agreement and transferred on 12/8/93. If further land is to be acquired it may impact on E-1, E-2, E-3 (SECV - electricity) and E-5 (Drainage).

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS319433	594 YAN YEAN ROAD	VOLUME 10152 FOLIO 832	E-1 (Carriageway) E-4 (Carriageway) E-2 (Drainage) E-3 (Carriageway)	E-4	SECTION 173	YES	Lot 1 PS319433Q	Planning Permit No. 65.13/4259 issued on 27/4/1993 by Nillumbik Shire Council for a two lot subdivision. Condition 3 requires the owner to enter into a Section 173 Agreement (11/10/1993). The agreement restricts fencing along the reserve boundary; except with the prior approval in writing of the Responsible Authority no fill shall be placed or caused to be placed on any reserve shown on the Plan of Subdivision. The agreement removes access to Yan Yeon Road (Easements E1 and E4) following the creation of an easement to provide access to the rear of the lots.
1\PS324157	515-525 YAN YEAN ROAD	VOLUME 10354 FOLIO 110	E-1 (Drainage)	NO	SECTION 173	YES	Lot 1 PS324157J	Planning Permit No. 65.13/4381 issued on 3/8/1993 by Nillumbik Shire Council. Condition 3 requires the owner to enter into a Section 173 Agreement. The agreement prohibits access to Yan Yeon Road and except with the prior approval in writing of the Responsible Authority no fill shall be placed or caused to be placed on any reserve shown on the Plan of Subdivision. The Plan of Subdivision contains Reserve No.1 for use of Shire of Nillumbik (Landscaping), except with the prior written consent of Council. Plan of Subdivision No. 324157J includes the creation of Reserve No. 1 Landscaping for use of Shire of Nillumbik.

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS324947	405 YAN YEAN ROAD	VOLUME 10187 FOLIO 695	E-1 (Carriageway) E-2 (Supply of Electricity & Carriageway) E-3 (Carriageway)	E-1 & E-2	SECTION 173 & SECTION 10 (1) LAND ACQUISITION AND COMPENSATION ACT 1986 - ROADS CORPORATION	NO INFORMATION ON AGREEMENT IN TITLE	lot 1 PS324947C	Plan No. PS 324947C includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on E-1 (carriageway) E-2 (electricity supplies & carriageway) and E-3 (carriageway).
1\PS326039	625-627 YAN YEAN ROAD	VOLUME 10165 FOLIO 635	E-4 - (Way) E-1 (Way)	E-4	SECTION 173	YES	Lot1 PS326039E	Planning Permit No. 65.13/4520 issued on 23/12/93 by Shire of Diamond Valley for a two lot subdivision. Condition 2 requires the owner to enter into a Section 173 Agreement (18/2/1994). The agreement restricts access onto Yan Yeon Road by providing access via Ashley Road. The agreement can be amended with the written consent of Council. Plantation Reserve No.1 vested with the Shire of Diamond Valley, E-5 Drainage (Shire of Diamond Valley) would be impacted by land acquisition.
1\PS326106	17 VISTA COURT	VOLUME 10160 FOLIO 386	NO	NO	SECTION 173	NO		

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS333777	2 BANNONS LANE	VOLUME 10264 FOLIO 959	NO	NO	SECTION 173	YES	Lot 1 PS333777A	Planning Permit No. 65.13/4502 issued on 20/6/1994 by Nillumbik Shire Council for a 24 lot subdivision. Condition 3 requires the owner to enter into a Section 173 Agreement (17/5/1995). The agreement contains building and works within a building envelope and sewer effluent building envelopes, except with the approval of Council no native vegetation shall be removed from this land unless it is for the safe and proper construction of roads and associated services. The endorsed plan did not provide for direct access to Yan Yean Road. Reserve No 1. And 2. along Yan Yean Road are vested with Nillumbik Shire Council for use of landscaping.

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS345234	503 YAN YEAN ROAD	VOLUME 10310 FOLIO 316	E-1 (Drainage)	NO	SECTION 173	YES	Lot1 PS345234Y	Planning Permit No. E95/358 issued on 29/04/1996 by Nillumbik Shire Council for a two lot subdivision. Condition 2 requires the owner to enter into a Section 173 Agreement (07/01/1997). The agreement contains 1) the need for the owner must grant a carriageway in favour of the owners of the abutting land to enable the owners of the abutting land access and egress to Yan Yeans Road 2) relocation of easements in accordance with <i>Yarrambat Local Structure Plan</i> . There is a severance clause within the agreement. Plan No. 345234Y includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services) and E-3 (carriageway).
1\PS411617	810 YAN YEAN ROAD	VOLUME 10356 FOLIO 147	E-2 & E-3 (Power Line-Eastern Energy)	E-2	NO	NO		Plan No. PS411617Q shows if further land is to be acquired it may impact on E-2 (Power Line-Eastern Energy)

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS413287	499 YAN YEAN ROAD	VOLUME 10365 FOLIO 100	E-2 (Drainage-Shire of Nillumbik)	NO	SECTION 173	YES	Lot 1 PS413287	Planning Permit No. 910/2001/05P issued on 13/09/2002 by Nillumbik Shire Council for building and works (construction of a Dwelling) and amended 24/09/2002 by Nillumbik Shire Council. Condition 5 requires the owner to enter into a section 173 agreement (06/02/2003). the agreement outlines within 4 years of the date of this permit, the existing dwelling on the land must be completely removed from the site. Plan no. 413287Y included a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on E-3 (water, electricity, gas supplies, drainage, sewage and telephone services).
1\PS424624	577 YAN YEAN ROAD	VOLUME 10522 FOLIO 315	NO	NO	SECTION 173	YES	Lot 1 PS424624V	Planning Permit No. 414/1999/14P issued on 03/02/2000 by Nillumbik Shire Council. Condition 6 requires the registered lots 1 & 2 of PS424624V enter into Section 173 agreement outlining 1) Building and works to be contained in building envelope 2) without written consent all building and works must be kept at least 4 metres away from trunks of trees. There is a severance clause within the agreement. Plan no.424624V includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping.

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1PS429717	10 NORTH OATLANDS ROAD	VOLUME 10621 FOLIO 864	NO	NO	SECTION 173	YES	Lot 1 LP55631	Planning Permit No. 289/2000/14P issued on the 06/06/2001 by Nillumbik Shire Council. Condition 3 requires the registered owner to enter into a section 173 outlining 1) building and works to be contained in the specific envelopes and there is written professional advice that proposed works have responded to potential risk to human health and flora and fauna 2) all works to be kept at least 4 metres away from tree trunks except with prior written consent from the council & any indigenous trees located within Lot to be marked for retention unless otherwise consented by the Council. Plan No.429717L includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on E-1 (water, electricity, gas supplies, drainage and telephone services).

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS431894	3 YOUNGS ROAD	VOLUME 10708 FOLIO 761	E-3 (Drainage-Shire of Nillumbik)	E--3	SECTION 173	YES	Lot 1 PS431894Y	Planning Permit No. 765/1999/14P for a two lot subdivision was issued on 30/5/2000 and amended 15/8/02 by Nillumbik Shire Council. Condition 2 requires the owner to enter into a Section 173 Agreement (21/10/02). The agreement allows access to Yan Yean Road from lot 1 until the lot is developed in any way that requires planning approval under the <i>Planning and Environment Act 1987</i> . Once a planning permit is issued Council will withdraw consent to access Yan Yean Road and an alternative access from Youngs Road will be required. There is a severance clause within the agreement. Plan No. 431894Y includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on E-3 (Drainage) (Nillumbik Shire Council).

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS441306	513 YAN YEAN ROAD	VOLUME 10619 FOLIO 499	E-1 (Drainage) (Supply of Electricity & Water) E-2 (Supply of Electricity & Water) E-3 (Supply of Electricity & Water) E-4 (Drainage)	E2 (Supply of Electricity & Water)	SECTION 173	YES	Lot 1 LP128738	Planning Permit No. 547/2000/14P for a two lot subdivision was issued on 16/07/2001 by Nillumbik Shire Council. Condition 4 requires the owner to enter into a Section 173 Agreement (18/10/2001). The agreement outlines 1) no building or works outside the building envelope unless written consent from council 2) effluent envelope only used for sewage, sullage and effluent unless written consent from the council. There is a severance in this agreement. Plan No.441306 includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on A-1 carriageway that provides access to both lots.

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS449409	541 YAN YEAN ROAD	VOLUME 10737 FOLIO 365	E-3 & E-5 (Carriageway) E-4 (Drainage & Floodway - Nillumbik Shire Council)	E-3 (Carriageway)	SECTION 173	YES	Lot 1 LP 55631	Planning permit No. 108/2002/14P for a two lot subdivision that was issued 06/12/2002 by Nillumbik Shire Council. Condition 2 requires the owner to enter into a section 173 Agreement (23/01/2003). The agreement outlines without prior written consent of the council all building and works must be contained within the building envelope. The agreement also contains the need for written consent from the council for effluent disposal via wastewater treatment plant. Finally, a Landscape plan must be provided to the satisfaction of the council for a dwelling, this includes a 10 metre landscape screen along Yan Yeon Road and planting vegetation requirements. Plan No. 449409K includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services) and access for Lot 1 & 2 access to properties through E-3 & E-1 Carriageway shown on plan.
Portion A - 1\PS503260	840 YAN YEAN ROAD	VOLUME 11521 FOLIO 020	E-1 - E-8 (Powerline)	E-3 & E-5	SECTION 173	NO		

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1PS511111	6 YOUNGS ROAD	VOLUME 10759 FOLIO 556	NO	NO	SECTION 173	YES	Lot 1 LP 74130	Planning permit No. 58/2003/14P for a two lot subdivision that was issued 06/06/2003 by Nillumbik Shire Council. Condition 3 requires the owner to enter into a section 173 Agreement (14/07/2003). The agreement outlines without prior written consent from the council all building and works must be contained within the building envelope. The agreement also contains the need for written consent from the council for effluent disposal via wastewater treatment plant. Finally, a Landscape plan must be provided to the satisfaction of the Council prior to building and works happening on lot 1, including detailing any existing vegetation removal. Access to Lot 1 must be via Young Road. Plan No.5111115 includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact on E-2 utilities.

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
1\PS511151	488 IRONBARK ROAD	VOLUME 10993 FOLIO 697 (Lot 1)	NO	NO	SECTION 173	YES	Lot 1 LP523335	Planning permit No. 434/2003/14P for a two lot subdivision that was issued 22/07/2004 by Nillumbik Shire Council. Condition 2 requires the owner to enter into a Section 173 (05/11/2004). The agreement outlines without prior written consent from the council all future building and works must be contained within the building envelope. Finally, a Landscape plan must be provided to develop the subject site including a detail plan of a 30 metre wide area of landscaping along the western boundary. Plan PS51151E includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for plantation.
1\PS538907	469-475 IRONBARK ROAD	VOLUME 00963 FOLIO 416	NO	NO	NO	NO		
1\PS634356	411 YAN YEAN ROAD	VOLUME 11205 FOLIO 680	E-2 (Drainage - Nillumbik Shire Council)	E-2	SECTION 10 (1) LAND ACQUISITION AND COMPENSATION ACT 1986 - ROADS CORPORATION	YES	Lot 1 LP43702	Section 10 (1) <i>Land Acquisition and Compensation Act 1986</i> applies to this lot 1. Issued by the Roads Corporation (21/06/2017). Please see Public Acquisition Overlay. If further acquisition land is to be acquired it may impact on E-2 (shire of Nillumbik Shire Council - Drainage).
1\TP103928	835 YAN YEAN ROAD	VOLUME 09642 FOLIO 240	NO	NO	NO	NO		
1\TP106083	825 YAN YEAN ROAD	VOLUME 09667 FOLIO 175	NO	NO	NO	NO		

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1\TP121835	815 YAN YEAN ROAD	VOLUME 09444 FOLIO 265	NO	NO	NO	NO		
1\TP121837	571 YAN YEAN ROAD	VOLUME 09444 FOLIO 306	NO	NO	NO	NO		
1\TP139444	920 BRIDGE INN ROAD	VOLUME 09812 FOLIO 803	E-1 E-2 (Electricity cables)	E-2	NO	NO	Lot 1 TP139444Y	If further acquisition land is to be acquired it may impact on E-1 & E-2 (The easements to State Electricity Commission of Victoria).
1\TP139445	910 BRIDGE INN ROAD	VOLUME 09812 FOLIO 804	E-1 E-2 (Electricity cables)	E-2	NO	NO	Lot 1 TP139445W	If further acquisition land is to be acquired it may impact on E-1 & E-2 (The easements to State Electricity Commission of Victoria).
1\TP165458	552 YAN YEAN ROAD	VOLUME 09246 FOLIO 475	NO	NO	NO	NO		
1\TP228353	649 YAN YEAN ROAD	VOLUME 04126 FOLIO 024	NO	NO	NO	NO		
1\TP229986	460-482 YAN YEAN ROAD	VOLUME 06926 FOLIO 052	NO	NO	NO	NO		
1\TP338012	391-399 YAN YEAN ROAD	VOLUME 08173 FOLIO 785	NO	NO	SECTION 10 (1) LAND ACQUISITION AND COMPENSATION ACT 1986 - ROADS CORPORATION	YES	Lot 1 TP338012V	Section 10 (1) Land Acquisition and Compensation Act 1986 applies to this lot. Issued by the Roads Corporation (21/06/2017). Please see Public Acquisition Overlay.
1\TP395983	552 YAN YEAN ROAD	VOLUME 05697 FOLIO 201	NO	NO	NO	NO		

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1\TP412316	428 YAN YEAN ROAD	VOLUME 04569 FOLIO 605	NO	NO	SECTION 10 (1) LAND ACQUISITION AND COMPENSATION ACT 1986 - ROADS CORPORATION	YES	Lot 1 TP412316M	Section 10 (1) <i>Land Acquisition and Compensation Act 1986</i> applies to this lot. Issued by the Roads Corporation (21/06/2017). Please see Public Acquisition Overlay.
1\TP424114	657 YAN YEAN ROAD	VOLUME 03898 FOLIO 405	NO	NO	NO	NO		
1\TP447635	920A YAN YEAN ROAD	VOLUME 07594 FOLIO 166	NO	NO	NO	NO		
1\TP820165	900 YAN YEAN ROAD	VOLUME 05891 FOLIO 076	NO	NO	NO	NO		
1\TP876436	501 YAN YEAN ROAD	VOLUME 08575 FOLIO 725	NO	NO	NO	NO		
1\TP894395	649 & 651 YAN YEAN ROAD	VOLUME 03914 FOLIO 731	NO	NO	NO	NO		
1\TP894911	657 YAN YEAN ROAD	VOLUME 03898 FOLIO 406	NO	NO	NO	NO		
1\TP913747	935 YAN YEAN ROAD	VOLUME 08442 FOLIO 047	E-1 E-2 (State Electricity Commission of Victoria)	E-1	NO	NO	Lot 1 TP913747P	If further acquisition land is to be acquired it may impact on E-1 (The easements to State Electricity Commission of Victoria).
1~13\PP3183	807 YAN YEAN ROAD	VOLUME 11758 FOLIO 671	NO	NO	NO	NO		

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1A/PS332404	614 YAN YEAN ROAD	VOLUME 10298 FOLIO 007	E-1 (Way) E-4 (Way, Electricity Supply - Eastern Energy Ltd & Supply of water, telephone and Gas) E-2 E-3	E-1 & E-4	SECTION 173	YES	Lot 1A PS332404	Planning permit No. E95/357 for a two stage, four lot subdivision that was issued 21/11/1995 by Nillumbik Shire Council. Condition 3 requires the owner to enter into a section 173 Agreement (26/10/1996). The agreement outlines without prior written consent from the council 1) all building and works must be contained within the building envelope 2) written consent from the Council for works within effluent disposal area and for removal of trees 3) written consent from the Council to remove native vegetation outside the building envelope 4) access via Yan Yeans must cease once an alternative access /egress to Youngs Road is provided. Plan No. 332404X includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for Plantation. If further land is to be acquired it may impact on E-1 (Way) E-4 (Way, Electricity Supply - Eastern Energy Ltd & Supply of water, telephone and Gas).
1D~4/PP3955	915 YAN YEAN ROAD (Crown land)	VOLUME 11787 FOLIO 013	NO	NO	NO	NO		
2/LP34910	540-550 YAN YEAN ROAD	VOLUME 08230 FOLIO 978	NO	NO	NO	NO		
2/LP43702	423-437 YAN YEAN ROAD	VOLUME 08553 FOLIO 079	NO	NO	NO	NO		

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2/LP44022	467-491 YAN YEAN ROAD	VOLUME 08831 FOLIO 645	NO	NO	NO	NO		
2/LP65401	880 YAN YEAN ROAD	VOLUME 08943 FOLIO 242	NO	NO	NO	NO		
2/LP80247	770 YAN YEAN ROAD	VOLUME 08786 FOLIO 257	NO	NO	NO	NO		
2/LP88194	430-438 YAN YEAN ROAD	VOLUME 09017 FOLIO 055	NO	NO	SECTION 10 (1) LAND ACQUISITION AND COMPENSATION ACT 1986 - ROADS CORPORATION	YES	Lot 2 LP88194	Section 10 (1) <i>Land Acquisition and Compensation Act 1986</i> applies to this lot. Issued by the Roads Corporation (21/06/2017). Please see Public Acquisition Overlay.
2/PS319433	590 YAN YEAN ROAD	VOLUME 10152 FOLIO 833	E-1 E-4 - Carriageway	NO	SECTION 173	YES		Section 173 (29/10/1993) condition 3 outlines the owner shall not object to any planning permit for the removal of easements E-1 & E-4 shown on Plan PS319433Q. A 6 month notification period is required from the authority to the owner for removal of access to/from Yan Yeon Road. Plan No. PS319433Q includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is to be acquired it may impact E-1 & E-4 (Carriageway).

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2/PS320992	4 RAOUL COURT	VOLUME 10219 FOLIO 595	E-2 (Way)	NO	SECTION 173	YES	Lot 2 PS320992T	Planning Permit No. 65.13/4231 for an eight lot subdivision and single detached houses was issued by Shire of Diamond Valley. Condition 3 requires the owner to enter into a Section 173 Agreement (5/9/94). The agreement allows temporary access to Yan Yean Road until alternative access provided. It includes a building and effluent envelope and a 30 metre building setback from Yan Yean Road. Plan No. 320992 includes the a 1.5 metre tree reserve vested with Shire of Diamond Valley for landscaping.
2/PS326039	629 YAN YEAN ROAD	VOLUME 10165 FOLIO 636	E-5 (Drainage - Shire of Diamond Valley)	E-5	SECTION 173	YES	Lot 2 326039E	Planning Permit No. 65.13/4520 issued on 23/12/93 by Shire of Diamond Valley for a two lot subdivision. Condition 2 requires the owner to enter into a Section 173 Agreement (11/2/1994). The agreement restricts access onto Yan Yean Road by providing access via Ashley Road. The agreement can be amended with the written consent of Council. Plantation Reserve No.1 vested with the Shire of Diamond Valley along the frontage of Yan Yean Road E-5 and E-2 Drainage (Shire of Diamond Valley) would be impacted by land acquisition.

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2/PS345234	507A YAN YEAN ROAD	VOLUME 10310 FOLIO 317	E-3 (Carriageway, Way, Drainage, Sewerage & Supply of Water, Electricity, Telephone Services and Gas) E-4 (Carriageway, Way, Drainage, Sewerage & Supply of Water, Electricity, Telephone Services and Gas)	E-3	SECTION 173	YES	Lot 1 PS345234Y	Planning Permit No. 65.13/4458 for a two lot subdivision was issued on 16/11/93 by Nillumbik Shire Council. Condition 3 requires the owner to enter into a Section 173 Agreement (4/12/96). The agreement allows access to Yan Yeon Road from adjoining lots via E-3 and E-4. There is a severance clause within the agreement. Plan No. 345234Y includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping along Yan Yeon Road.
2/PS413287	493 YAN YEAN ROAD	VOLUME 10365 FOLIO 101	E-2 (Drainage)	NO	NO	NO		

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2/PS424624	579 YAN YEAN ROAD	VOLUME 10522 FOLIO 316	E-2 (Carriageway)	E-2	SECTION 173	YES	Lot 2 PS424624V	Planning Permit No. 414/1999/14P (amended) for a boundary Re-alignment and 2 lot subdivision was issued 03/02/2000. Condition 6 requires the owner to enter into a section 173 Agreement (10/04/2000). The agreement outlines 1) all building and works to be contained within the building envelopes 2) without written consent from the council all building and works to be kept at least 4 metres clear of the trunk of trees designated for retention 3) No trees shall be lopped, topped or removed without prior approval from the council. There is a severance clause within the agreement. Plan No. 424624V includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for Landscaping. If further land is to be acquired it may impact on E-2 (carriageway) which provides access for Lot A, 1 and 2.
2/PS449409	553 YAN YEAN ROAD	VOLUME 10737 FOLIO 366	E-3 E-4 (Carriageway)	E-3	SECTION 173	NO	Lot 2 PS449409K	Section 173 dictates to Lot 1.

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2/PS511151	476 IRONBARK ROAD	VOLUME 10993 FOLIO 698 (Lot2)	NO	NO	SECTION 173	YES	Lot 2 LP523335	Planning Permit No. 434/2003/14P for a two lot subdivision that was issued 22/07/2004 by Nillumbik Shire Council. Condition 2 requires the owner to enter into a Section 173 (05/11/2004). The agreement outlines 1) without prior written consent from the council all building and works to be contained within the building/Effluent envelopes 2) landscape plan detailing a 30 metre wide area of landscaping along the western Boundary of the subject site. There is a severance clause within the agreement. Plan No. 511151E includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for Landscaping.
2/TP876436	501 YAN YEAN ROAD	VOLUME 08575 FOLIO 725	NO	NO	NO	NO		

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2A/PS332404	602 YAN YEAN ROAD	VOLUME 10298 FOLIO 008	E-2 E-5	NO	SECTION 173	YES	Lot 2 PS332404X	Planning Permit No. E95/357 for a two stage, four lot subdivision that was issued 21/11/1995 by Nillumbik Shire Council. Condition 3 requires the owner to enter into a section 173 Agreement (26/10/1996). The agreement outlines without prior written consent from the council 1) all building and works must be contained within the building envelope 2) written consent from the Council for works within effluent disposal area and for removal of trees 3) written consent from the Council to remove native vegetation outside the building envelope 4) Access via Yan Yeon Road must cease once an alternative access /egress to Youngs Road is provided. Plan No. 332404X includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for Plantation. If further land is to be acquired it may impact on E-1 (Way) E-4 (water, electricity, gas supplies, drainage and telephone services).
3/LP11297	615-623 YAN YEAN ROAD	VOLUME 08403 FOLIO 796	NO	NO	NO	NO		
3/LP43702	439-465 YAN YEAN ROAD	VOLUME 08196 FOLIO 992	NO	NO	SECTION 10(1) LAND ACQUISITION AND COMPENSATION ACT 1986 ROADS CORPORATION	YES	Lot 3 LP43702	Section 10 (1) <i>Land Acquisition and Compensation Act 1986</i> applies to this lot. Issued by the Roads Corporation (21/06/2017). Please see Public Acquisition Overlay.

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
3\PS320992	3 RAOUL COURT	VOLUME 10219 FOLIO 596	E-4	YES	YES	YES	Lot 3 PS320992T	Section 173 (05/09/1994) outlines Lot 3 cease use of the temporary carriageway easement for the purpose of gaining access from the owner's land to Yan Yean Road. Plan No. 320992T includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for Plantation. If further land is to be acquired it may impact on E-4 (Way, water, electricity, gas supplies, drainage and telephone services).
3\PS444782	581 YAN YEAN ROAD	VOLUME 10724 FOLIO 908	E-1 (Carriageway)	YES	SECTION 173	NO	Lot 3 PS444782N	Section 173 dictates to Lot 4 rather than Lot 3.
4\PS444782	585 YAN YEAN ROAD	VOLUME 10724 FOLIO 909	E-1 (Carriageway)	YES	SECTION 173	YES	Lot 4 PS424624	Planning Permit No. 225/2002/14P for a two lot subdivision issued 19/08/2002 by Nillumbik Shire Council. Condition 2 requires the owner to enter into a section 173 Agreement 19/07/2002. the agreement outlines that the owner agrees 1) except with written consent from the council no building or works outside the building envelope 2) effluent disposal must be via a wastewater treatment plant 3) the construction of a dwelling must be accompanied by a landscape plan that requires enhancement of vegetation along Yan Yean road frontage. There is a severance clause within the agreement. If further land is to be acquired it may impact on E-1 (carriageway) and A-1 (carriageway, water, electricity, gas supplies, drainage and telephone services).

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
6\LP52838	945 YAN YEAN ROAD	VOLUME 08546 FOLIO 356	E-1 & E-2 (transmission of Electricity - State of Victoria)	E1 & E-2	NO	NO	Lot 6 PS052838	Plan No. PS052838 Includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-1 & E-2 (Electricity - State of Electricity Commission of Victoria).
8\LP42418	514-524 YAN YEAN ROAD	VOLUME 08342 FOLIO 910	NO	NO	NO	NO		
9\LP42418	500-512 YAN YEAN ROAD	VOLUME 08384 FOLIO 593	NO	NO	NO	NO		
D\PS632417	31A YELLOW BRICK ROAD	VOLUME 11394 FOLIO 534	NO	NO	NO	NO		
R2\PS326106	1 VISTA COURT	VOLUME 10160 FOLIO 398	NO	NO	NO	NO		
R2\PS632417	25A YELLOW BRICK ROAD	VOLUME 11394 FOLIO 536	NO TITLE	NO TITLE	NO TITLE	NO TITLE		
R2\PS632417	25A YELLOW BRICK ROAD	VOLUME 11394 FOLIO 536	NO TITLE	NO TITLE	NO TITLE	NO TITLE		
R3\PS326106	18 VISTA COURT	VOLUME 10160 FOLIO 399	NO	NO	NO	NO		
RES1\PS319433	488A-620A YAN YEAN ROAD	VOLUME 10152 FOLIO 834	E-1 (Carriageway)	E-1	NO	NO	Lot RES1	Plan No. PS319433Q Includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-1 (Carriageway).

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
RES1\PS3209 92	626A YAN YEAN ROAD	VOLUME 10219 FOLIO 598	NO	NO	SECTION 173	NO	Lot RES1 PS320992T	Plan No. PS320992T Includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-1 (water, electricity supply, telephone and gas services) E-3 (way, water, electricity, gas supplies, drainage and telephone services) and E-4 (way, water, electricity, gas supplies, drainage and telephone services).
RES1\PS3241 57	513A YAN YEAN ROAD	VOLUME 10354 FOLIO 112	E-2 (Drainage)	E-2	SECTION 173	NO	Lot RES1 PS320992T	Plan No. PS 441 306P includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).
RES1\PS3249 47	405A YAN YEAN ROAD	VOLUME 10187 FOLIO 697	E-3 (Carrageway)	E-3	SECTION 173 & SECTION 10 (1) <i>LAND ACQUISITION AND COMPENSATION ACT 1986 - ROADS CORPORATION</i>	NO INFORMATION ON AGREEMENT IN TITLE	Lot RES1 PS324947C	Plan No. 324947C includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. A Section 10 (1) <i>Land Acquisition and Compensation Act 1986</i> applies to this lot. Issued by the Roads Corporation (21/06/2017). Please see Public Acquisition Overlay.
RES1\PS3260 39	625-629 YAN YEAN ROAD	VOLUME 10165 FOLIO 637	E-2 (Drainage and Supply of Water, Electricity, Telephone and Gas) E-3 (Way, Drainage and Supply of Water, Electricity, Telephone and Gas)	E-2 & E-3	NO	NO	Lot RES1 PS326039E	Plan No. 326039E includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 & E-3 (water, electricity, gas supplies, drainage and telephone services).

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RES1\PS326106	1 VISTA COURT	VOLUME 10160 FOLIO 394	NO	NO	NO	NO		
RES1\PS329383	1 GOLF LINKS DRIVE	VOLUME 10178 FOLIO 330	NO	NO	SECTION 173	NO	Lot RES1 PS333777A	Plan No. PS333777A includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping.
RES1\PS332404	614 YAN YEAN ROAD	VOLUME 10280 FOLIO 739	E-1 (Way) E-4(Way, Electricity Supply - Eastern Energy Ltd & Supply of water, telephone and Gas)	E1 & E4	SECTION 173	NO	Lot RES1 PS324157J	Plan No. PS324157J includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-1 (way), E-4 (way, water, electricity, gas supplies, drainage and telephone services).
RES1\PS333777	1 GOLF LINKS ROAD	VOLUME 11371 FOLIO 261	NO	NO	SECTION 173	NO INFORMATION ON AGREEMENT IN TITLE	Lot RES1 PS333777A	Plan No. 333777A includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping.
RES1\PS345234	503A YAN YEAN ROAD	VOLUME 10310 FOLIO 318	E-2 (Drainage, Sewage & Supply of Water, Electricity, Telephone Services and Gas)	E-2	SECTION 173	NO	Lot RES1 PS345234Y	Plan No. PS324947C includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).
RES1\PS413287	493-499A YAN YEAN ROAD	VOLUME 10365 FOLIO 102	E-1 (Way, Drainage and supply of Water, Electricity, Telephone and Gas)	E-1	NO	NO	Lot RES1 PS413287Y	Plan No. PS413287Y includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
RES1\PS424624	583 YAN YEAN ROAD	VOLUME 10522 FOLIO 318	E-1 (Carriageway, Drainage, Sewerage and Supply of Water, Electricity, Telephone Services and gas) E-3 (Drainage, Sewerage and supply of Water, Electricity, Telephone Services and Gas)	E1 & E3	NO	NO	Lot RES1 PS424624V	Plan No. PS424624V includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).
RES1\PS429717	541A YAN YEAN ROAD	VOLUME 10621 FOLIO 866	E-1 (Drainage, Sewerage, Electricity, water telephone services and gas)	E-1	NO	NO	Lot RES1 PS429717L	Plan No. PS429717L includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).
RES1\PS431894	626A YAN YEAN ROAD	VOLUME 11371 FOLIO 437	NO	NO	NO	NO		
RES1\PS441306	513A YAN YEAN ROAD	VOLUME 10619 FOLIO 549	NO	NO	NO	NO		
RES1\PS449409	541A YAN YEAN ROAD	VOLUME 11371 FOLIO 496	E-2 (Drainage, Sewage & Water, Electricity, Telephone Services and Gas)	E-2	NO	NO	Lot RES1 PS44909K	Plan No. PS449409K includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).

SPI	ADDRESS	COT	EASEMENTS ON PLAN	EASEMENT IMPACT ON PROJECT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 AGREEMENT	REGISTERED RESTRICTIVE COVENANT OR SECTION 173 IMPACT ON PROJECT	LOT NUMBER	AGREEMENT TERMS
RES1\PS5111	488A-620A YAN YEAN ROAD	VOLUME 11371 FOLIO 564	E-2 (Drainage, Sewage & Water, Electricity, Telephone Services and Gas)	E-2	NO	NO	Lot RES1 PS511115	Plan No. PS511111S includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).
RES1\PS5111	488A-620A YAN YEAN ROAD	VOLUME 11371 FOLIO 955	E-2 (Drainage, Sewage & Water, Electricity, Telephone Services and Gas)	E-2	NO	NO	Lot RES1	Plan includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).
RES1\PS520106	2W WERTHER WAY	VOLUME 10854 FOLIO 054	E-1 (Carriageway - Whittlesea City Council)	E-1	NO	NO	Lot RES1 PS520106J	Plan No. PS520106J includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping. If further land is acquired it may impact on E-2 (water, electricity, gas supplies, drainage and telephone services).
RES1\PS532862	805W YAN YEAN ROAD	VOLUME 10982 FOLIO 601	NO	NO	NO	NO		
RES2\PS333777	1 GOLF LINKS DRIVE	VOLUME 10457 FOLIO 521	NO	NO	SECTION 173	NO	Lot RES2 PS333777A	Plan PS333777A includes a 1.5 metre tree reserve vested with Nillumbik Shire Council for landscaping.

APPENDIX H

PUBLIC ACQUISITION OVERLAY MAPS



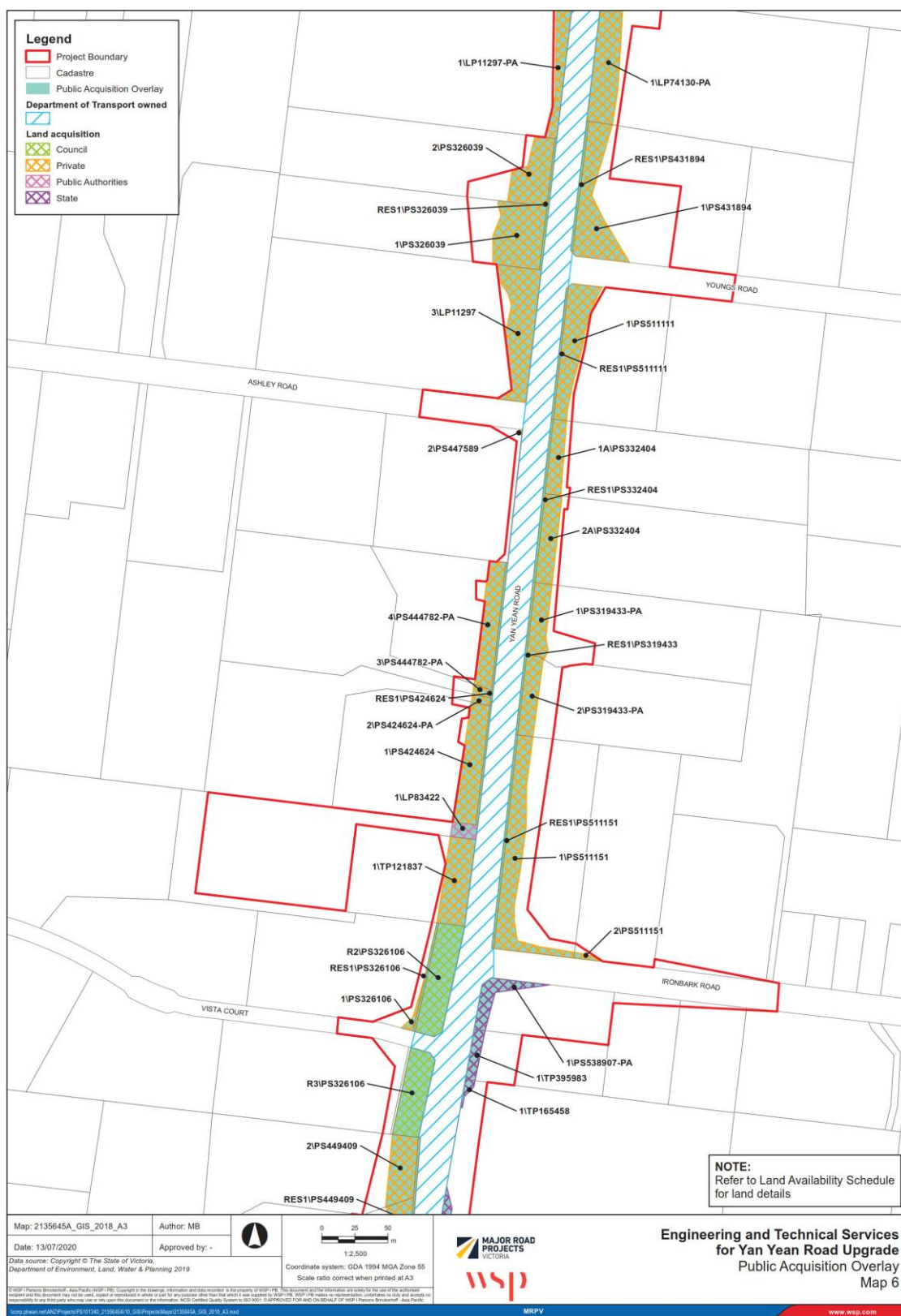


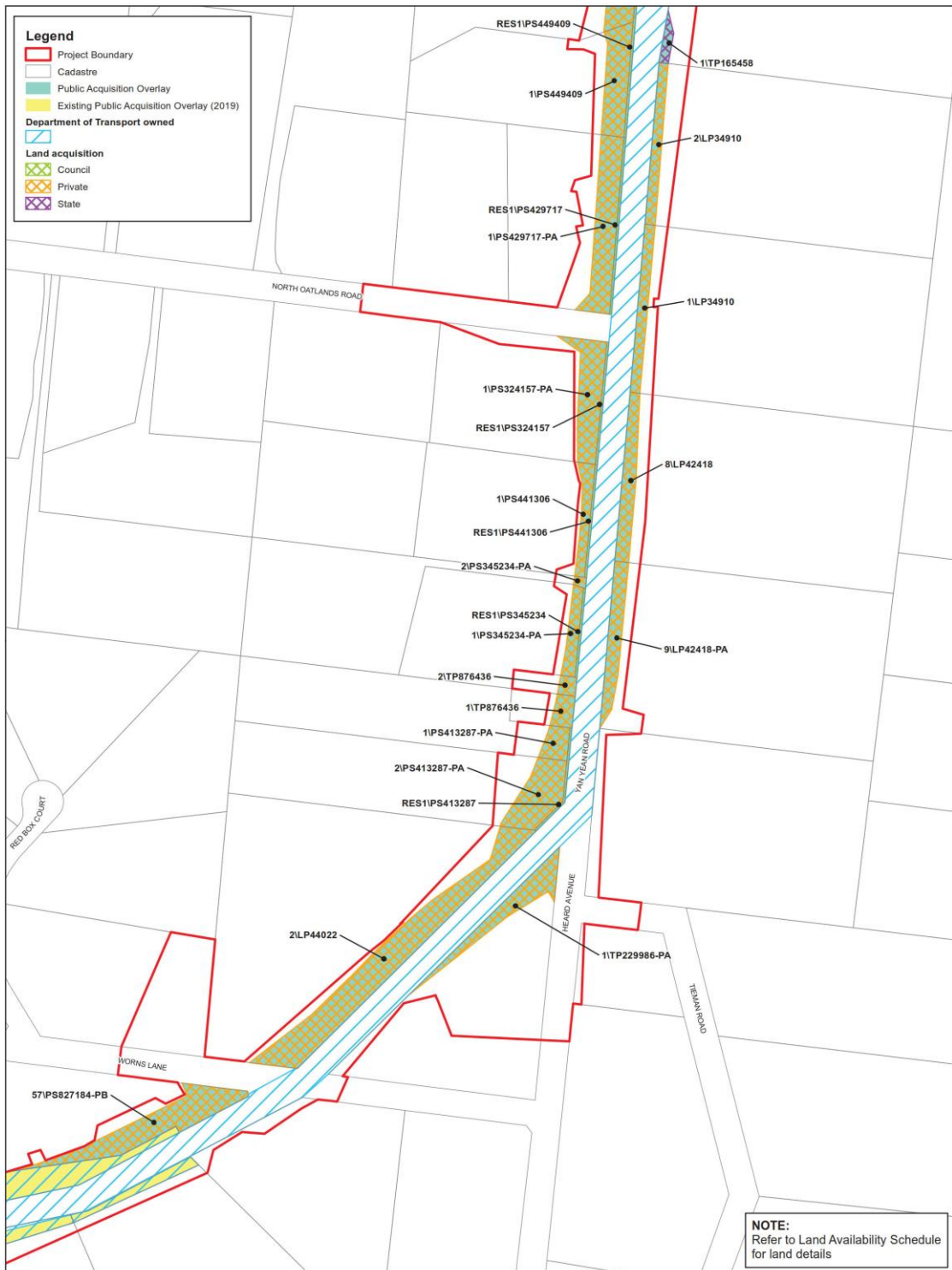


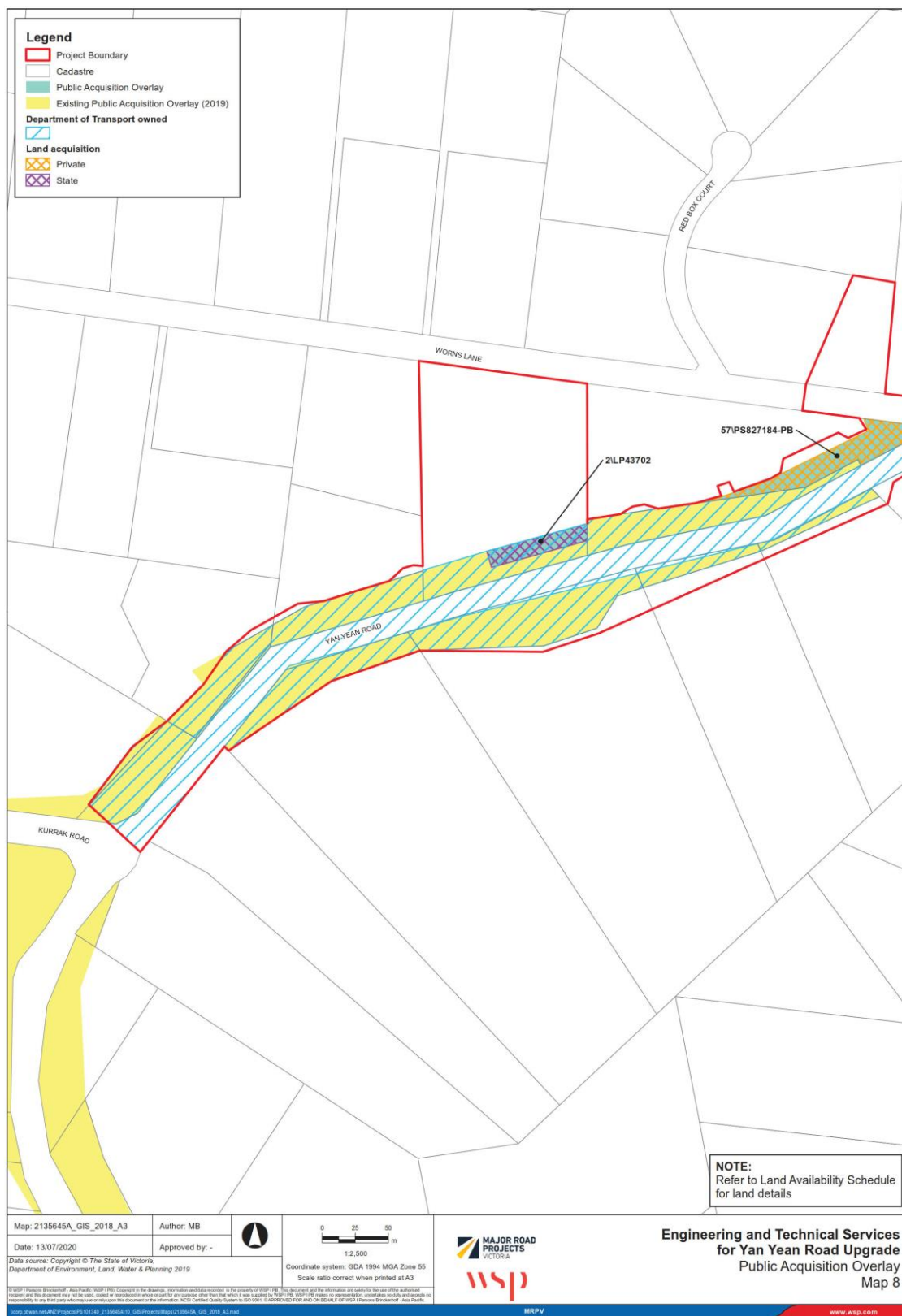












APPENDIX I

LAND ACQUISITION TABLE



Table I.1 Yan Yean Road Upgrade Project – Proposed Land Acquisition (new PAO)

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
1\LP11297	635-643 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	757	19796	Residential
1\LP34910	526-538 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	868	24981	Residential
1\LP65401	870 YAN YEAN ROAD DOREEN 3754	Doreen	Private	752	4408	Residential
1\LP74130	638-646 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1835	20329	Residential
1\LP80247	780 YAN YEAN ROAD DOREEN 3754	Yarrambat	Private	2385	30001	Residential
1\LP83422	573 & 573A & 573B YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	215	12439	Public utility
1\LP94920	830 YAN YEAN ROAD DOREEN 3754	Doreen	Private	5768	81401	Residential
1\PS305570	790A YAN YEAN ROAD DOREEN 3754	Yarrambat	Private	6673	125458	Residential
1\PS314043	25 DOCTORS GULLY ROAD DOREEN 3754	Doreen	Private	44155	1037110	Rural
1\PS319433	594 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	734	9803	Residential
1\PS320992	5 RAOUL COURT YARRAMBAT 3091	Yarrambat	Private	34	11861	Residential
1\PS324157	9 NORTH OATLANDS ROAD YARRAMBAT 3091	Yarrambat	Private	1508	10515	Residential
1\PS326039	625-627 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1736	9768	Residential

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
1\PS326106	17 VISTA COURT YARRAMBAT 3091	Yarrambat	Private	95	10001	Residential
1\PS329383	3 GOLF LINKS DRIVE YARRAMBAT 3091	Yarrambat	Private	66	8926	Residential
1\PS333777	2 BANNONS LANE YARRAMBAT 3091	Yarrambat	Private	413	10071	Residential
1\PS345234	503 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	538	8963	Residential
1\PS411617	810 YAN YEAN ROAD DOREEN 3754	Doreen	Private	5304	40849	Residential
1\PS413287	499 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	418	10212	Residential
1\PS424624	577 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1452	10051	Residential
1\PS429717	10 NORTH OATLANDS ROAD YARRAMBAT 3091	Yarrambat	Private	2048	9964	Residential
1\PS431894	3 YOUNGS ROAD YARRAMBAT 3091	Yarrambat	Private	1972	12534	Residential
1\PS441306	513 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	609	10030	Residential
1\PS449409	541 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1337	10014	Vet
1\PS503260	840 YAN YEAN ROAD DOREEN 3754	Doreen	Private	7297	163637	Education
1\PS511111	6 YOUNGS ROAD YARRAMBAT 3091	Yarrambat	Private	1568	8073	Childcare
1\PS511151	488 IRONBARK ROAD YARRAMBAT	Yarrambat	Private	2328	9982	Residential

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
1\PS538907	552 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	DoE	584	32982	Education
1\TP121835	815 YAN YEAN ROAD DOREEN 3754	Doreen	Private	941	32523	Residential
1\TP121837	571 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1343	6008	Residential
1\TP139444	920 BRIDGE INN ROAD DOREEN 3754	Doreen	Private	1473	20240	Residential
1\TP165458	552 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	DoE	287	32982	Education
1\TP228353	649-651 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	17560	1044596	Reserve
1\TP229986	460-482 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1209	13626	Residential
1\TP395983	552 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	DoE	300	32982	Education
1\TP424114	657 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	2957	120985	Reserve
1\TP820165	900 YAN YEAN ROAD DOREEN 3754	Doreen	Private	16225	41864	Residential
1\TP876436	501 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Telstra	327	1668	Residential
1\TP894395	649-651 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	5032	1044596	Reserve
1\TP894911	657 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	4868	120985	Reserve

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
1~13\PP3183	807 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	DELWP	5774	196952	Reserve
1A\PS332404	614 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	621	11208	Residential
1D~4\PP3955	915 YAN YEAN ROAD DOREEN 3754	Doreen	WCC	9438	20491	Reserve
2\LP34910	540-550 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	812	24377	Residential
2\LP43702	423-437 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	DoT	2946	19483	Residential
2\LP44022	467-491 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	4504	27514	Residential
2\LP65401	880 YAN YEAN ROAD DOREEN 3754	Doreen	Private	9786	321465	Rural
2\LP80247	770 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1587	21547	Residential
2\PS319433	590 YAN YEAN ROAD YARRAMBAT	Yarrambat	Private	743	10008	Residential
2\PS320992	4 RAOUL COURT YARRAMBAT 3091	Yarrambat	Private	337	11546	Residential
2\PS326039	629 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1199	10528	Residential
2\PS345234	507 & 507A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	62	11000	Residential
2\PS413287	493 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1407	12296	Residential

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
2\PS424624	579 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	85	10884	Residential
2\PS447589	605 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	6	10427	Kennels
2\PS449409	553 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1011	10177	Childcare
2\PS511151	476 IRONBARK ROAD YARRAMBAT	Yarrambat	Private	65	10010	Residential
2\TP876436	501 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Telstra	168	1668	Residential
2A\PS332404	602 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	877	13734	Residential
3\LP11297	615-623 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	2090	20741	Worship
3\LP80247	15 LAURIE STREET YARRAMBAT 3091	Yarrambat	Private	13	20138	Residential
3\PS320992	3 RAOUL COURT YARRAMBAT 3091	Yarrambat	Private	1054	12061	Residential
3\PS444782	581 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	92	12803	Residential
4\PS444782	585 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1231	7435	Residential
57\PS827184	439-465 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	2321	15954	Residential
7\LP52838	915 BRIDGE INN ROAD DOREEN 3754	Doreen	Private	1074	15908	Residential

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
8\LP42418	514-524 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1127	24592	Residential
9\LP42418	500-512 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	Private	1067	24069	Residential
91\PS811813	13 ORCHARD ROAD DOREEN 3754	Doreen	Private	16	382	Residential
92\PS811813	11 ORCHARD ROAD DOREEN 3754	Doreen	Private	31	400	Residential
93\PS811813	9 ORCHARD ROAD DOREEN 3754	Doreen	Private	31	400	Residential
94\PS811813	7 ORCHARD ROAD DOREEN 3754	Doreen	Private	31	400	Residential
95\PS811813	5 ORCHARD ROAD DOREEN 3754	Doreen	Private	32	446	Residential
R2\PS326106	1 VISTA COURT YARRAMBAT 3091	Yarrambat	NSC	1695	1695	Reserve
R3\PS326106	18 VISTA COURT YARRAMBAT 3091	Yarrambat	NSC	1296	1296	Reserve
RES1\PS319433	488A-620A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	166	746	Reserve
RES1\PS320992	626A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	312	642	Reserve
RES1\PS324157	513A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	132	254	Reserve
RES1\PS326039	625-629 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	150	150	Reserve
RES1\PS326106	1 VISTA COURT YARRAMBAT 3091	Yarrambat	NSC	126	1821	Reserve

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
RES1\PS329383	1 GOLF LINKS DRIVE YARRAMBAT 3091	Yarrambat	NSC	96	668	Reserve
RES1\PS332404	488A-620A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	186	746	Reserve
RES1\PS333777	1 GOLF LINKS DRIVE YARRAMBAT 3091	Yarrambat	NSC	153	668	Reserve
RES1\PS345234	503A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	94	94	Reserve
RES1\PS413287	493-499A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	124	124	Reserve
RES1\PS424624	583 YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	266	281	Reserve
RES1\PS429717	541A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	192	386	Reserve
RES1\PS431894	626A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	149	642	Reserve
RES1\PS441306	513A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	122	254	Reserve
RES1\PS449409	541A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	194	386	Reserve
RES1\PS511111	488A-620A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	150	746	Reserve
RES1\PS511151	488A-620A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	244	746	Reserve
RES1\PS520106	2W WERTHER WAY DOREEN 3754	Doreen	NSC	3683	13667	Reserve

SPI	ADDRESS		LANDOWNER STATUS	AMOUNT OF LAND TO BE ACQUIRED (SQM)	TOTAL LAND AREA (SQM)	CURRENT LAND USE
RES1\PS811813	24W ELDERFIELD CIRCUIT DOREEN 3754	Doreen	WCC	567	2906	Reserve
RES2\PS329383	626A YAN YEAN ROAD YARRAMBAT 3091	Yarrambat	NSC	181	642	Reserve
RES2\PS333777	1 GOLF LINKS DRIVE YARRAMBAT 3091	Yarrambat	NSC	419	668	Reserve

APPENDIX J

PROJECT DESCRIPTION





PART 1 INTRODUCTORY CHAPTERS

5 Project Description

SECTION	PAGE	SECTION	PAGE
5.1 Introduction	5.2	5.3.6 Sustainability and climate change	5.15
5.2 Project overview	5.2	5.3.7 Land acquisition	5.15
5.3 Project design	5.6	5.4 Project construction	5.16
5.3.1 Road design	5.6	5.4.1 Construction activities	5.16
5.3.2 Active transport design elements	5.13	5.4.2 Construction laydown areas	5.16
5.3.3 Utilities	5.14	5.4.3 Construction method	5.16
5.3.4 Drainage design	5.14	5.4.4 Working hours	5.17
5.3.5 Landscaping and urban design	5.15	5.5 Project operation and maintenance	5.17

5.1 Introduction

This chapter describes the proposed design, construction and operation of the duplication of Yan Yean Road between Kurrak Road and Bridge Inn Road (the Project). The chapter should be read in conjunction with Attachment VI *Map Book*, which contains detailed plans and drawings of key elements of the Project.

This Project description has been developed to provide an understanding of all components, processes and development stages of the Project to enable assessment of the Project's potential environmental effects. The description includes specific design elements to address the potential for the Project to generate adverse environmental effects and impacts.

5.2 Project overview

Yan Yean Road is a significant north-south arterial road servicing the Shire of Nillumbik and the City of Whittlesea, providing connectivity for the City of Whittlesea's growing suburbs of Doreen and Mernda to the townships of Plenty and Yarrambat. Yan Yean Road connects with major east-west arterials such as Bridge Inn Road, Kurrak Road and Diamond Creek Road and also provides a connection to employment and services in established neighbouring suburbs such as Greensborough and Diamond Creek.

Stage 1 of the Yan Yean Road upgrade (Diamond Creek Road to Kurrak Road) was completed in 2019, and construction on Stage 2 (the subject of this EES) is scheduled for completion by 2025.

The Project seeks to upgrade an existing road in hilly terrain, largely within the existing road reserve. The surrounding environment is characterised by low density residential and rural living areas such as farmland and agricultural areas, with the suburb of Doreen experiencing rapid change from rural living to higher density residential developments. The Project alignment and immediate surrounds intersect a range of land uses including residential, open space, rural living, commercial and education.

Key land uses along the alignment include Yarrambat Park and the Yarrambat Park Golf Course, Plenty Valley Christian College, Yarrambat Primary School, St Macarius Coptic Orthodox Church and the Doreen business precinct.

Terms used in this project description

Carriageway: lanes where traffic would be travelling, plus shoulders and auxiliary lanes

Cross section: shows the width of the road with the position and number of traffic lanes, medians, walking and cycling paths and footpaths

Cutting: ground excavation that is required to create a smooth base for construction of a road

Land parcel: the smallest unit of land able to be transferred within Victoria's cadastral system

Median: the area between two opposing carriageways

Mid-block: a section of road between key intersections

Outer edge / shoulder: the area next to a roadway that provides clearance between the roadway and roadside

Road reserve: all the area of land that is within the boundaries of a road

Roadside: any land that is within the boundaries of a road (other than the shoulders of the road) which is not a roadway or pathway

Roadway: the area of the public road that is open to or used by members of the public and is developed by a road authority for the driving or riding of motor vehicles

Signalised intersections: intersections controlled by traffic lights

The Project would duplicate a 5.5 kilometre section of Yan Yean Road between Kurrak Road and Bridge Inn Road, increasing the existing two lanes to four lanes (comprising two lanes in each direction). The design speed along Yan Yean Road within the extent of the project area is 70 kilometres per hour, with the exception of north of Bridge Inn Road where the design speed is 80 kilometres per hour. This is consistent with existing speed limits. The design for the Project assessed in this EES has 3.5-metre-wide lanes, with the majority of the Project using a central 2.2 metre-wide median. This design was adopted due to various constraints: road safety issues, steep and rolling terrain, high cut and fill batters and subsequent retaining walls at certain locations.

The design also seeks to limit impacts to existing properties, local accesses and trees along Yan Yean Road. The existing road alignment has been retained due to constraints around the topography and land uses adjacent to the road corridor. The exception is at the Bridge Inn Road intersection, which would be shifted to the north east to retain two River Red Gums (referred to as the Doreen River Red Gums) and two businesses. The project area is shown in Figure 5.1 and key components of the Project are shown in Figure 5.2.

The Project includes:

- Two new roundabouts: one at Heard Avenue and one at Youngs Road
- Five new signalised intersections at Bannons Lane, Jorgensen Avenue, North Oatlands Road, Orchard Road and Bridge Inn Road
- Upgrades to one existing signalised intersection at Ironbark Road, including an additional right-hand turning lane, slip lane and traffic island
- New street lighting at all intersections, road signage and landscaping
- A new walking and cycling path on the western side and a footpath on the eastern side of Yan Yean Road, linking Diamond Creek to Doreen and improving safety and connectivity for pedestrians and cyclists
- Continuous safety barriers running along the Project's length, proposed in the median and behind outer kerbs along the mid-block sections of the carriageways
- A wide median between Bannons Lane and Jorgensen Avenue to provide for additional landscaping opportunities and potential avoidance of existing biodiversity values and large trees.



Figure 5.1 Project area

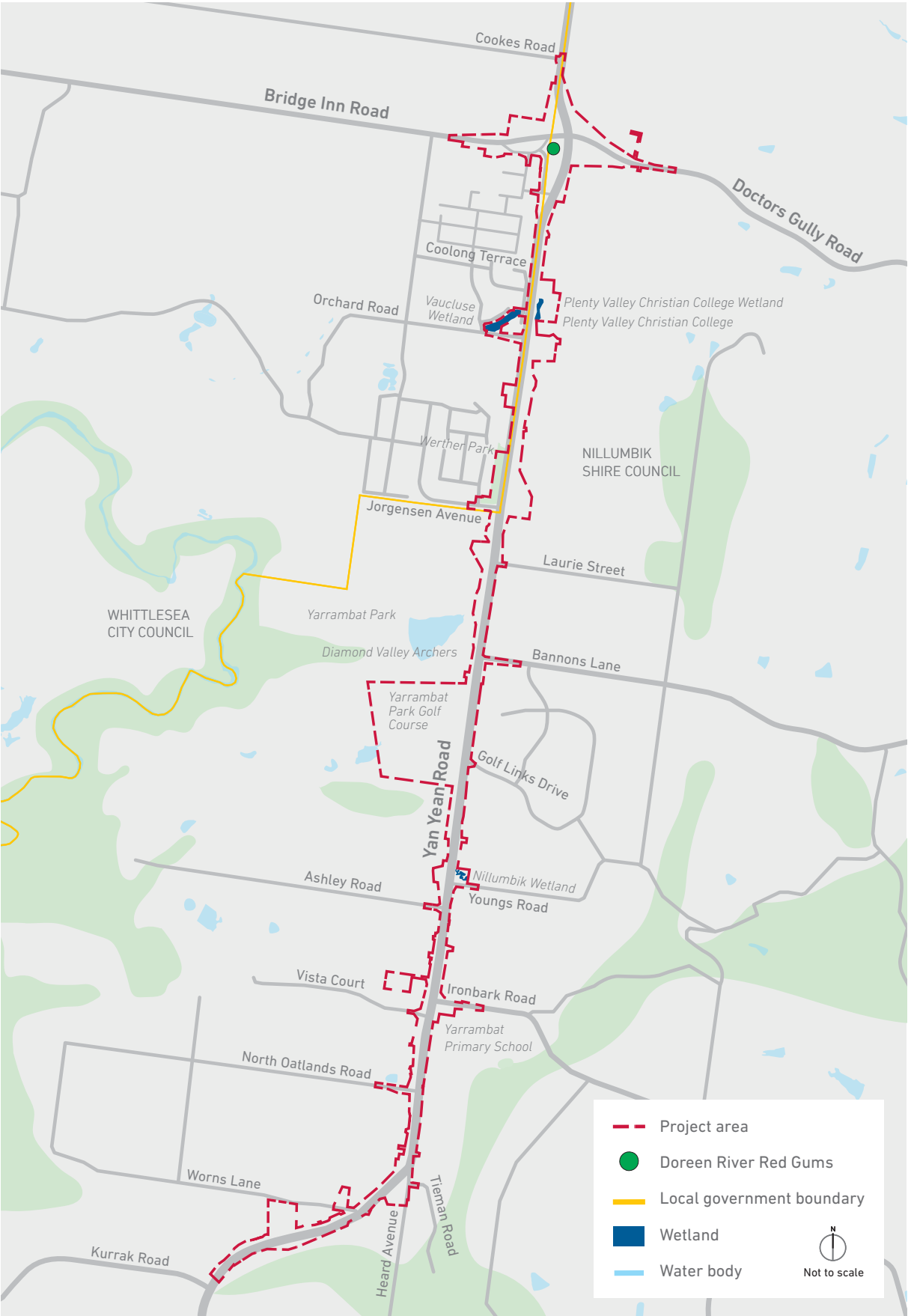
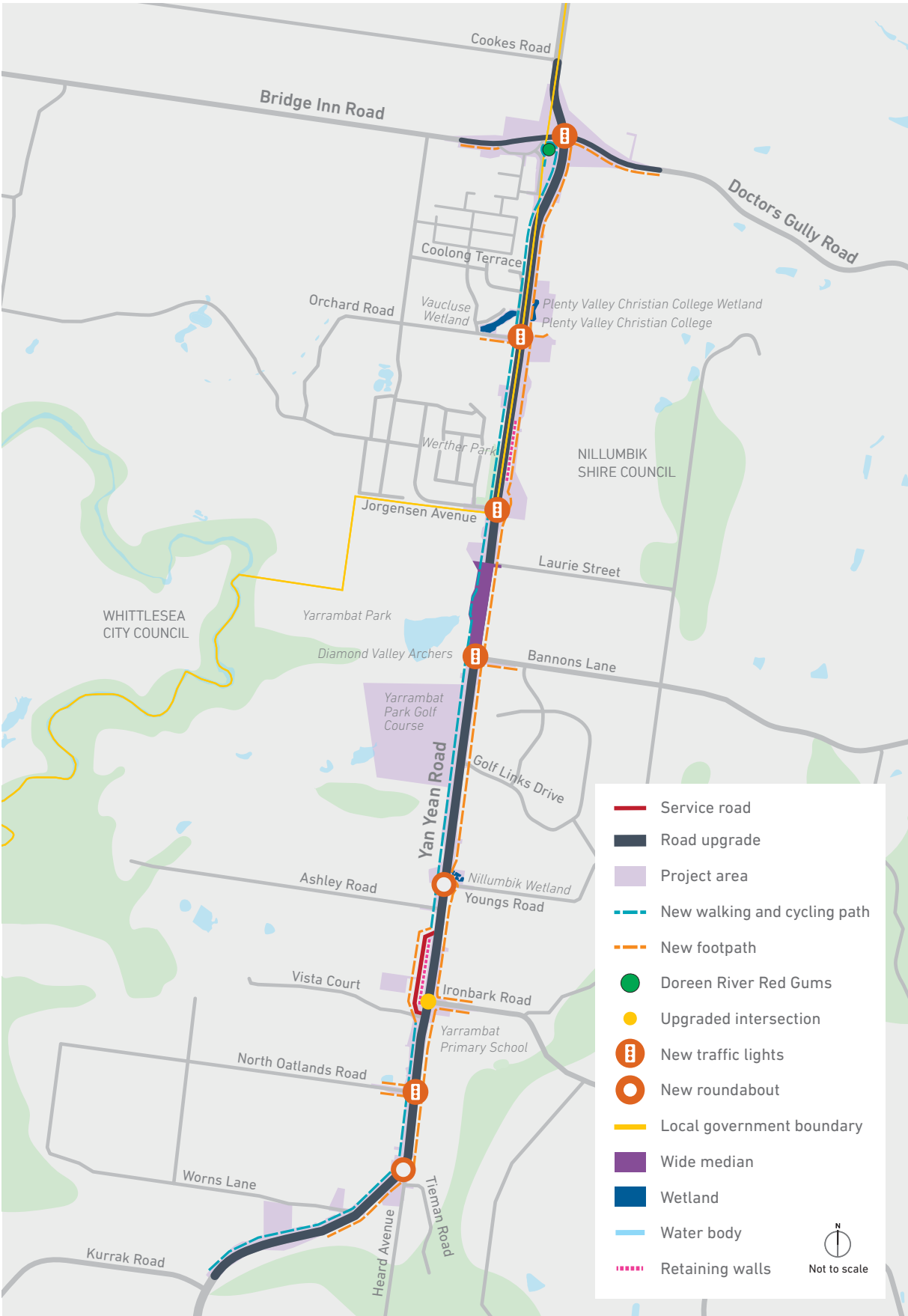


Figure 5.2 Key components of the Project



5.3 Project design

5.3.1 Road design

There are a number of elements to the road design of Yan Yean Road:

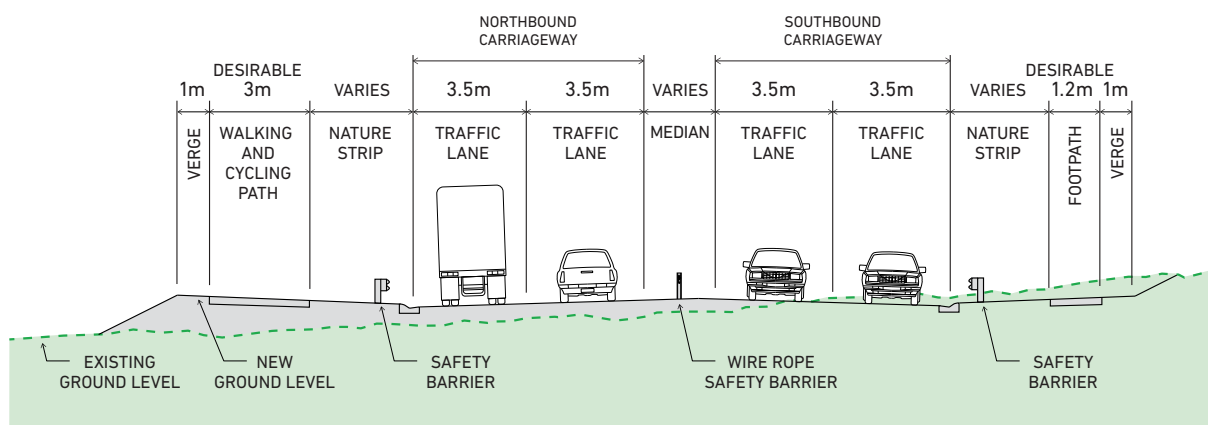
- Typical cross section
- Intersections
- Access
- Wide median
- Safety barriers
- Retaining walls
- Fencing
- Car parks
- Bus facilities.

Typical cross section

The following diagram indicates the typical cross section of the road design for the Project. At some locations along the alignment, such as intersections or roundabouts, this cross section would be slightly different and wider. Figure 5.3 shows the preferred mid-block cross section design, which allows for duplication with a 2.2 metre median with safety barriers.

The installation of safety barriers provides opportunities for tree planting in closer proximity to the road carriageway than would be otherwise permissible, in accordance with the Project's Landscape Strategy (Technical Report G). The total road reserve width along most of the proposed design is 24.2 metres increasing to 33 metres between Bannons Lane and Jorgensen Avenue to accommodate the widened median at this location. The current typical roadway width is eight metres.

Figure 5.3 Yan Yean Road preferred cross section design



For illustrative purposes only.


Intersection design

The scope of the Project includes modifications to a number of intersections. Signalised intersections are proposed to improve safety, provide U-turn opportunities and increase the capacity of existing intersections, and roundabouts are proposed to improve safety and provide larger U-turn opportunities. Intersection works include:

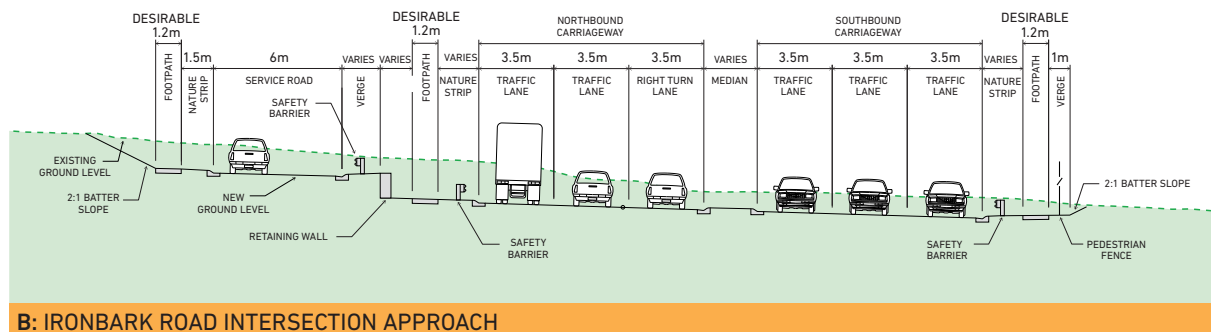
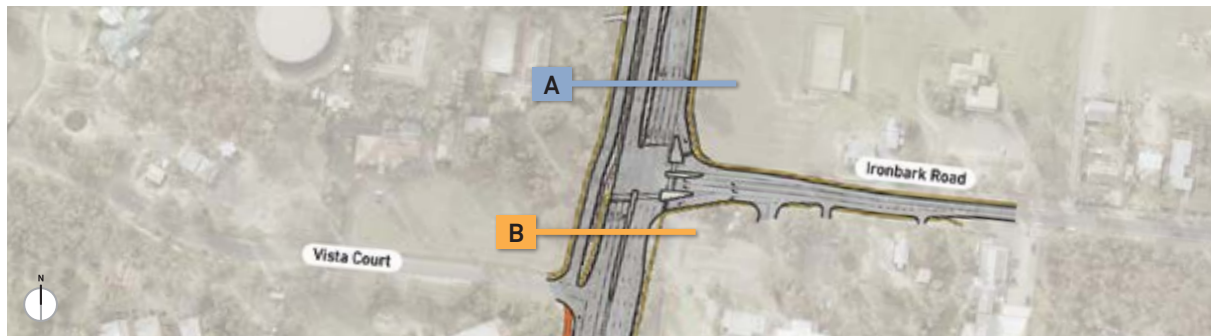
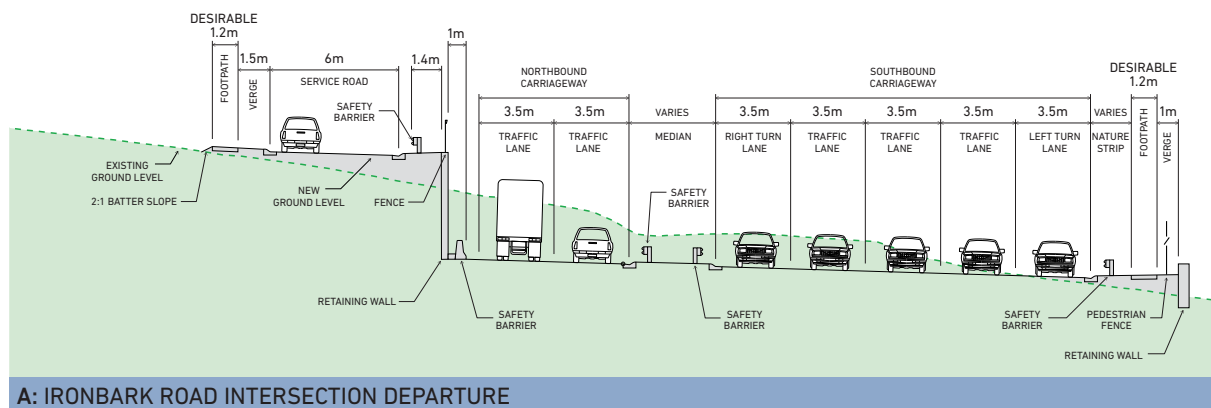
- Signalised intersections at North Oatlands Road, Ironbark Road (refer to Figure 5.4), Bannons Lane, Jorgensen Avenue, Orchard Road and Bridge Inn Road (refer to Figure 5.5)
- Roundabouts at Heard Avenue and Youngs Road
- Proposed left in / left out arrangements at all other intersections, including:
 - Yan Yean Road / Activity Way
 - Yan Yean Road / Laurie Street
 - Yan Yean Road / Golf Links Drive
 - Yan Yean Road / Ashley Road
 - Yan Yean Road / Service Road A exit (left out only)
 - Yan Yean Road / Vista Court
 - Yan Yean Road / Worns Lane
 - Yan Yean Road / 807 Yan Yean Road access
 - Yan Yean Road / Service Road B (between Kurrak Road and Worns Lane)
 - Residential properties and businesses along the alignment
- Auxiliary lanes provided for all left turns (and where applicable, right turns) from Yan Yean Road into key intersections to separate turning traffic from the main traffic flow to reduce collisions and improve the road capacity.

The project design at Bridge Inn Road would retain the two Doreen River Red Gums situated adjacent to the Bridge Inn Road and Yan Yean Road T-intersection and the General Store / former post office and Pet Supplies and Stockfeeds Store on the corner of Doctors Gully Road. It proposes shifting the whole intersection to the north-east corner of Yan Yean Road / Bridge Inn Road with two lanes in each direction.

The design at Bridge Inn Road has been refined following community consultation and in response to additional arboriculture advice on the Doreen River Red Gums, which are situated south-west of the proposed intersection (refer to Figure 5.5).

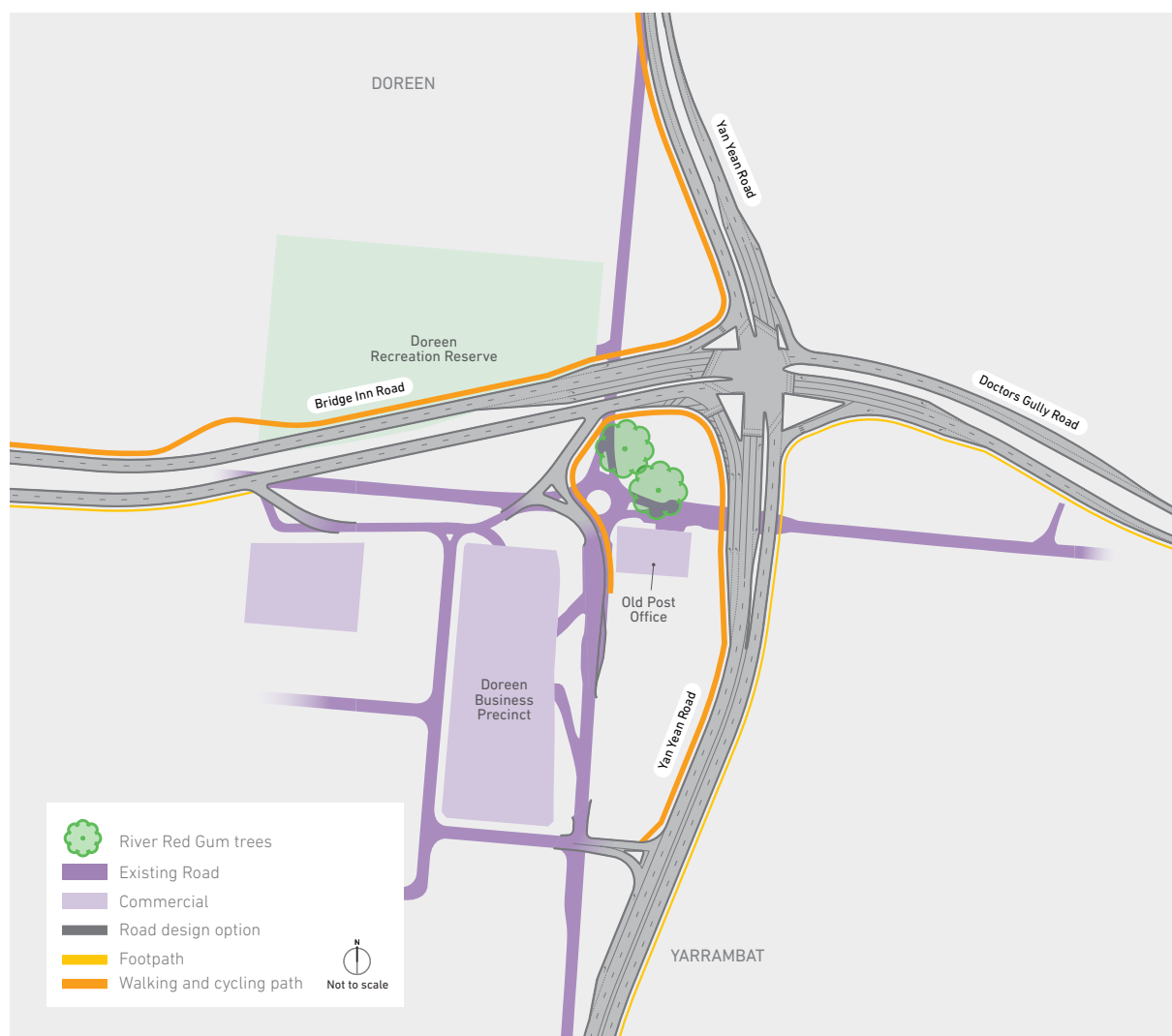


The project design at Bridge Inn Road would retain the two Doreen River Red Gums situated adjacent to the Bridge Inn Road – Yan Yean Road T-intersection and the General Store.

Figure 5.4 Typical signalised intersection cross section – Ironbark Road (northbound)

For illustrative purposes only.

Figure 5.5 Bridge Inn Road intersection design



For illustrative purposes only.

Access design

All existing accesses would be changed to left in / left out arrangements to allow for the installation of a centre median and safety barriers. U-turn lanes would be provided at the following locations to allow for the safe turning of vehicles wishing to travel in the opposite direction:

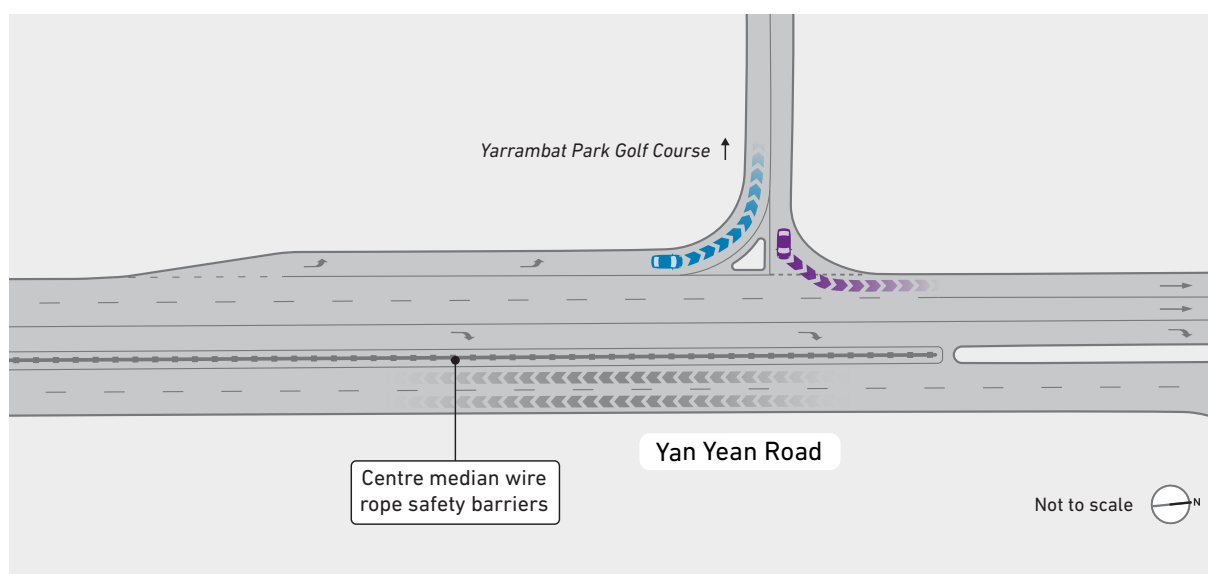
- Bridge Inn Road signalised intersection (cars only)
- Orchard Road signalised intersection (cars only)
- Jorgensen Avenue signalised intersection (cars only)
- Bannons Lane signalised intersection (cars only)
- Youngs Road roundabout (cars, cars with trailers / horse floats, semi-trailers and trucks)
- Ironbark Road signalised intersection (cars only)
- North Oatlands Road signalised intersection (cars only)
- Heard Avenue roundabout (cars, cars with trailers / horse floats, semi-trailers and trucks).

All existing Council approved property access and driveways are proposed to be maintained with minor tie-in works. Access for properties at the western side of Yan Yean Road from Vista Court to Ashley Road would be via a service road due to the steep grade and level differences between properties and Yan Yean Road (refer to Figure 5.4 and Attachment VI *Map Book*).

Access conditions at Yarrambat Primary School and Plenty Valley Christian College would be revised due to intersection upgrades impacting existing access and carpark arrangements.

The proposed design includes a left in / left out arrangement (refer to Figure 5.6) to the Yarrambat Park Golf Course.

Figure 5.6 Left in / left out arrangement – Access to Yarrambat Park Golf Course



For illustrative purposes only.

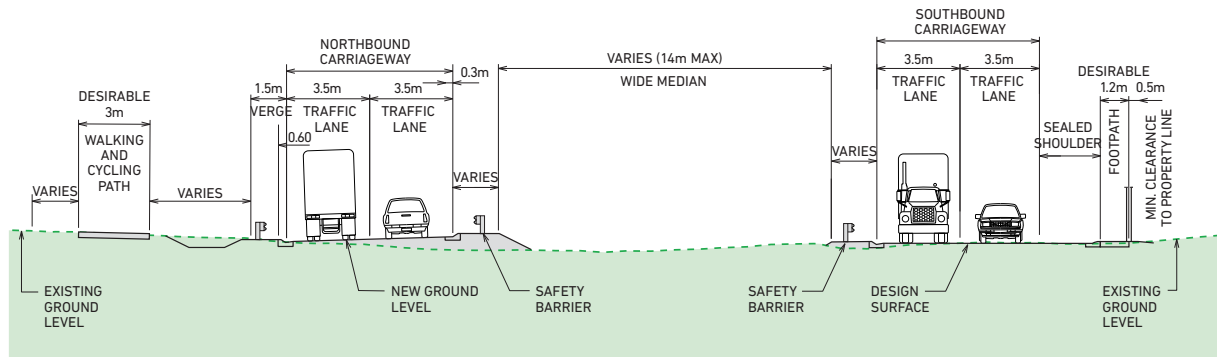
Wide median

A divided carriageway (boulevard design) increases the median width of Yan Yean Road from 2.2 metres to approximately 14 metres by realigning the northbound carriageway between Bannons Lane and Jorgensen Avenue (refer to Figure 5.7). The maximum road reserve width at this point would be approximately 33 metres, although the cross section would taper at either end to tie back into the standard cross section of 24.2 metres, as described above. A wider median at this location would provide for additional landscaping opportunities and potential avoidance of existing biodiversity values (including Matted Flax-lily) and large trees in accordance with the Project's Landscape Strategy (Technical Report G).

The southbound carriageway is aligned to follow the existing carriageway edge to retain the existing separation distance between driveways, residences and Yan Yean Road.

The wide median section of the road design tapers back to the standard cross section width at Bannons Lane. This allows the safe tapering of the road back to the standard road width while avoiding private land acquisition further south of the golf course.

Figure 5.7 Wide median cross section design



For illustrative purposes only.

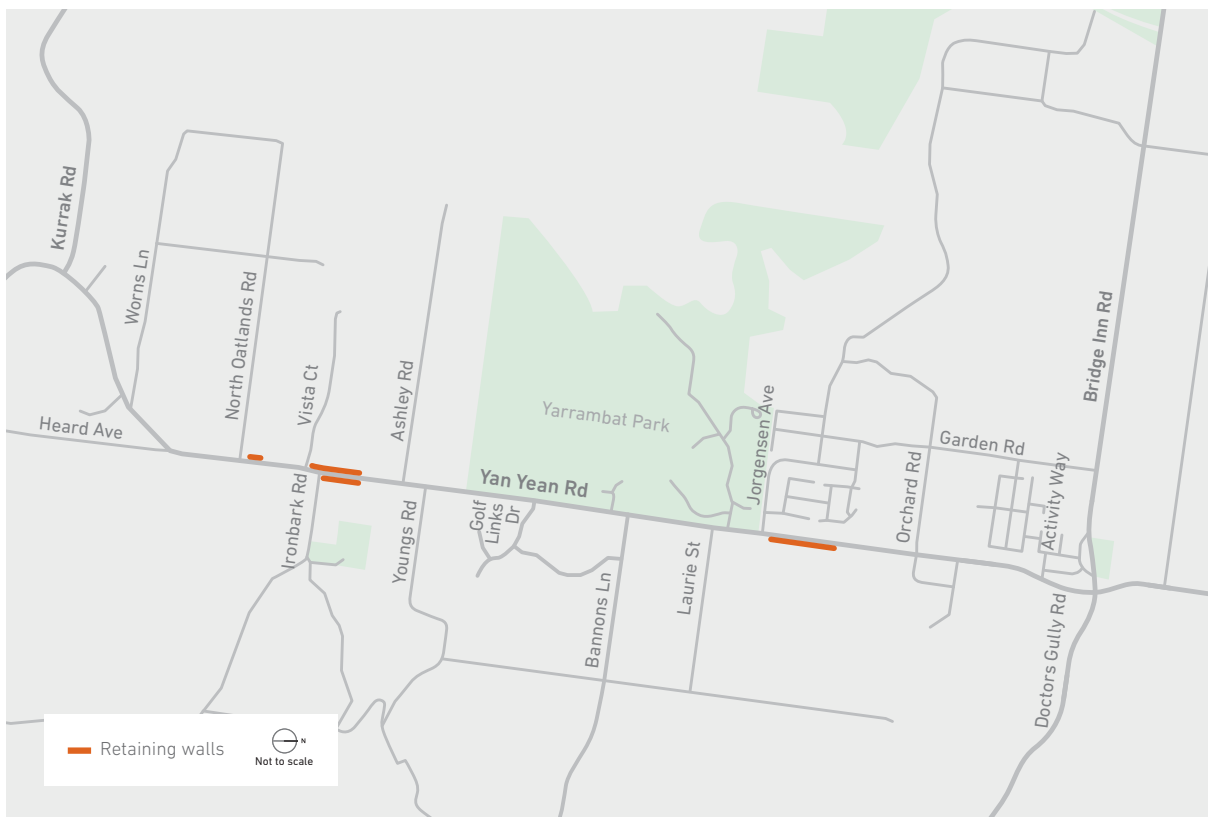
Safety barrier design

Continuous safety barriers are proposed in the median and behind most outer kerbs (where there are not intersections). Safety barriers would be installed at various setbacks from the kerb ranging from 0.6 to 1 metre, depending on factors such as speed limit, topography and barrier type. Safety barriers require a cleared area behind them to maintain the integrity of their effectiveness. This includes clearance from walking and cycling paths, as well as footpaths. Proposed safety barriers include guardrail, wire rope and concrete barriers if deemed required.

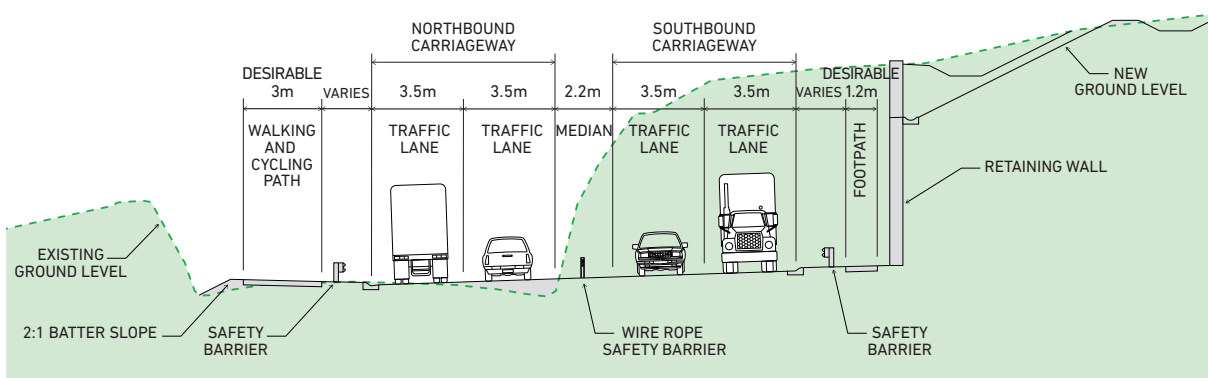
Retaining walls design

Retaining walls have been proposed at selected locations along Yan Yean Road to minimise the extent of land acquisition on adjacent properties, provide access to properties abutting Yan Yean Road, maximise the retention of existing trees and reduce the extent of cut earthworks. The design of retaining walls would be carried out in accordance with guidelines in the Project's Landscape Strategy (Technical Report G). Retaining walls are likely to be installed at the following locations (refer to Figure 5.8 and Figure 5.9):

- Between Service Road A and Yan Yean Road: a 270 metre long wall with an approximate maximum height of 3.6 metres. This retaining wall has been proposed to retain access to existing properties abutting Yan Yean Road and minimise impacts to existing trees
- At the north-east corner of Ironbark Road: a 230 metre long wall with an approximate maximum height of 2.4 metres. This retaining wall has been proposed to minimise the extent of land acquisition at the adjacent property
- North of North Oatlands Road along the western verge of Yan Yean Road: a 50 metre long wall with an approximate maximum height of 1.1 metres. This retaining wall has been proposed to minimise the extent of land acquisition at the adjacent property and minimise the impact to the existing driveway arrangement
- North of Jorgensen Avenue along the eastern verge of Yan Yean Road: a 220 metre long wall with an approximate maximum height of 8 metres. This retaining wall has been proposed to avoid impacting the existing telecommunication tower on the abutting property, maintain access to the adjacent property and telecommunication tower, maximise the retention of existing trees and reduce the extent of cut works.

Figure 5.8 Retaining wall locations

For illustrative purposes only.

Figure 5.9 Retaining wall cross section – north of Jorgensen Avenue intersection (northbound)

For illustrative purposes only.

Fencing design

The Project is required to ensure adequate safety measures are in place so that golf balls from Yarrambat Park Golf Course do not land on the walking and cycling path or road. This EES assumes that a 30-36 metre-high and 360 metre long fence along the edge of the golf course is included in the design to avoid golf ball collisions with pedestrians, cyclists or vehicles.

The proposed fence would incorporate elements to increase its visibility to Swift Parrot and other bird species. The alternative option to building a fence is to reconfigure golf course holes 1, 10 and 18 to increase their distance from the road and reduce the risk of golf balls landing on the new road and walking and cycling path to an acceptable level. This would not reduce the number of holes at the golf course.

A 1.8 metre timber paling fence has been designed to mitigate the risk of arrows from the Diamond Valley Archers facility affecting the road or walking and cycling path.

Plenty Valley Christian College and Yarrambat Primary School

Access to Plenty Valley Christian College and Yarrambat Primary School directly adjacent to the project area would be maintained during the Project's construction and operation. Some temporary arrangements may be required during construction to manage roadworks adjacent to the schools.

The Project would reconfigure and reinstate an existing car park at Plenty Valley Christian College. This includes a new access road to tie into the existing road. The dam at Plenty Valley Christian College would also require reconfiguration. This would be completed in collaboration with the school.

Land currently used by Yarrambat Primary School for informal car parking would require reconfiguration.

To facilitate these changes, partial land acquisition would be required along the frontage of both schools. This would be limited in extent and would not result in a long-term change to the existing land use; however, it would result in a permanent reduction in the land area on both school sites (refer to Attachment VI *Map Book*).

Bus facilities

Existing bus stops are proposed to be reinstated at the same location or within close proximity, in consultation with the Department of Transport and Public Transport Victoria. The project area allows for indentations around bus stops along the alignment if required.

5.3.2 Active transport design elements

Walking and cycling path and footpath

The design provides a walking and cycling path on the western side of Yan Yean Road in the following locations (refer to Figure 5.2):

- Adjacent to the northbound carriageway of Yan Yean Road from Kurrak Road to Bridge Inn Road, connecting to the existing walking and cycling path at both ends
- Adjacent to the eastbound carriageway of Bridge Inn Road, to be connected to existing walking and cycling paths.

Between Bannons Lane and Jorgensen Avenue, the walking and cycling path is realigned through Yarrambat Park and Shire of Nillumbik land to avoid the removal of more trees on the western side of Yan Yean Road. The walking and cycling path north of Jorgensen Avenue follows the existing footpath for the same purpose. The walking and cycling path would generally be three metres wide and would reduce slightly in width at various locations to allow the retention of trees.

In addition, a footpath, generally 1.2 metres wide, is proposed on the eastern side of Yan Yean Road in the following locations (refer to Figure 5.2):

- Adjacent to the southbound carriageway of Yan Yean Road from Bridge Inn Road to Kurrak Road to connect into the existing footpath
- Adjacent to the northbound carriageway of Yan Yean Road, along Service Road A from Vista Court to Ashley Road to connect to the proposed walking and cycling path extents
- Along Doctors Gully Road to Yan Yean Road to connect into the existing footpath.

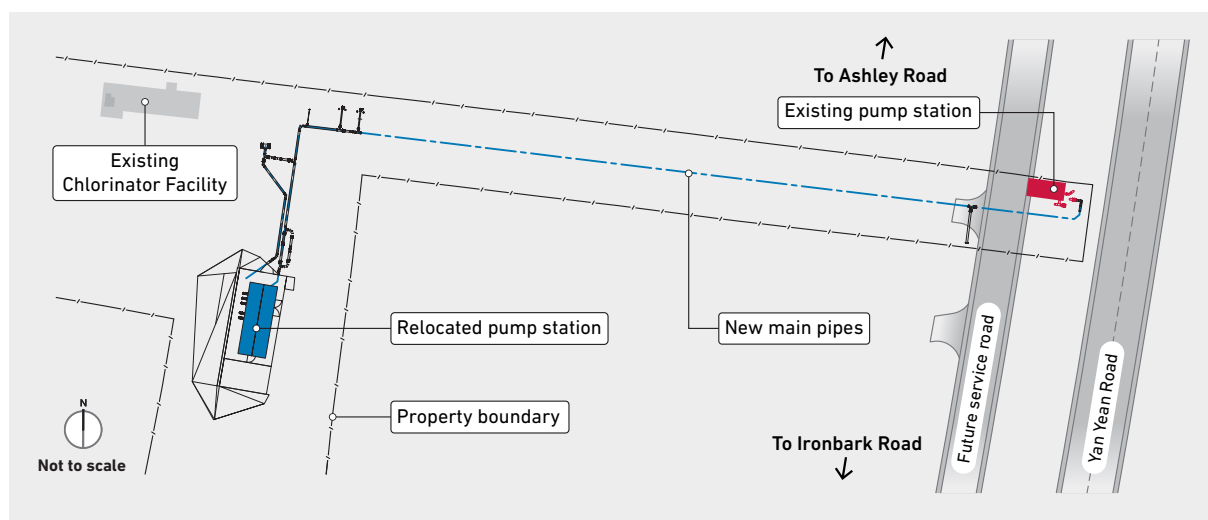
5.3.3 Utilities

New utility service upgrades, relocations and protection works may be required along the length of the Project. Where utility services cannot be avoided, protection / relocation / diversion works would occur adjacent to the proposed road pavement. Relocation of power lines along the alignment is anticipated to involve a combination of above ground and underground power. Works associated with existing water mains, sewer, gas and telecommunications assets may also require relocation and/or diversion adjacent to the road pavement. As such, a minimum allowance of five metres from the outermost construction extent (toe / top of batter, retaining wall, etc.) has been made to allow for potential utility upgrades and service relocations within the project area.

Relocation of Yarra Valley Water pump station

The project area includes a Yarra Valley Water pump station, near Ironbark Road on the western side of the existing Yan Yean Road, which the Project may be required to relocate. The tank may be re-located and new connecting infrastructure installed, all on existing Yarra Valley Water land. Refer to Figure 5.10 for the indicative relocation plan. MRPV continue to investigate design opportunities that could avoid the requirement to relocate the pump station.

Figure 5.10 Yarrambat pump station relocation indicative plan



For illustrative purposes only.

5.3.4 Drainage design

New drainage works, upgrades and relocations would occur along the length of the Project. Drainage along the alignment has been developed based on a flood model and expected outfall locations (which were determined by existing topography); however, the Project is also required to comply with water sensitive urban design (WSUD) requirements from Melbourne Water. This approach aims to make better use of stormwater in urban areas and reduce the harm it causes to the natural water cycle, rivers and creeks. Meeting Melbourne Water's requirements is likely to comprise grassed swale drains (where practicable), detention basins and water treatment basins.

The project area provides for a minimum 10 metres offset from the top of each drainage swale to allow for construction. In areas where drainage swales are not required, a minimum allowance of five metres from the outermost construction extent (toe / top of batter, retaining wall, etc.) has been provided in the project area to allow adequate construction space. The Project would coordinate closely with local schools to ensure the functionality of existing car parks and outdoor playing fields is maintained if these areas are impacted by drainage works.

Detention basin sites for surface water management have also been allowed for within the project area in proximity to Worns Lane, Heard Avenue, Youngs Road, Orchard Road (Melbourne Water wetland) and Bridge Inn Road.

5.3.5 Landscaping and urban design

A Landscape Strategy (Technical Report G) has been developed in consultation with Councils and other key stakeholders to ensure that the Project fits sensitively into the built, natural and cultural environment of Doreen and Yarrambat. The strategy would ensure that landscaping undertaken as part of the Project is well designed and contributes to the character and functioning of the Yan Yean Road corridor and the surrounding area, as well as to the accessibility and connectivity of people within the wider region and community. The Project would provide new and reinstated landscapes that are appropriate to the local conditions and consistent with the existing varied character of the area. Wherever possible, the Project would provide opportunities to increase canopy cover and improve amenity in the public realm.

The Landscape Strategy provides overarching principles to guide the Project landscape design, with a particular focus on minimising impacts on trees along the road corridor. Planting typologies have been considered to enhance the experience of drivers, pedestrians and cyclists, provide visual interest, screen infrastructure elements, improve habitat values and provide subtle wayfinding clues. Planting adjacent to the shared path would provide shelter and shade to improve user amenity. The activation of remnant open space would be explored to provide increased amenity to the local community where feasible.

5.3.6 Sustainability and climate change

MRPV is committed to delivering projects that optimise social, economic and environmental outcomes over the long term. To fulfil this commitment, MRPV would ensure:

- Sustainability risks and opportunities are identified and refined into project-appropriate performance objectives and requirements
- Delivery partners are monitored to ensure achievement of sustainability performance objectives and requirements
- Project sustainability performance is measured, verified and publicly reported on.

Key sustainability opportunities for the Project include:

- Ensuring the Project is resilient to the challenges of climate change by preparing and implementing a climate risk assessment and adaptation plan
- Optimising the use of recycled content in infrastructure materials
- Reducing greenhouse gas emissions, material lifecycle impacts and waste generation during the Project's construction and operation
- Protecting and enhancing the built, natural and cultural environment within and adjacent to the project area.

5.3.7 Land acquisition

The existing road corridor is not of sufficient width to accommodate the duplication and supporting infrastructure such as service roads, walking and cycling path and drainage. The Project would require the partial or full acquisition of 96 parcels of land. In most cases, partial acquisition of the land would be required along the frontages of landholdings.

This acquisition would be limited in extent and would not result in a long-term change in the existing land use, but it would result in a permanent reduction in the land area on those land parcels.

The land acquisition process would be undertaken in accordance with the *Land Acquisition and Compensation Act 1986* and would include consultation with affected landowners. Compensation would be provided for all land acquired for the Project. Refer to Attachment VI *Map Book* for the proposed Public Acquisition Overlay (PAO).

The landowner status of proposed land acquisition for the Project includes:

- Shire of Nillumbik: 24 land parcels
- City of Whittlesea: four land parcels
- Private: 60 land parcels
- Public Authorities / State: eight land parcels.

5.4 Project construction

5.4.1 Construction activities

Construction details would be subject to further refinement as the Project progresses; however, any changes to the activities and requirements outlined below would need to be in accordance with the Environmental Performance Requirements (EPRs) set out in Chapter 12 *Environmental Management Framework*.

Proposed construction activities would be standard road construction activities to be undertaken in accordance with the EPRs for the Project.

Site establishment would involve tree clearance and vegetation lopping and removal within the project area, establishment of construction site compounds, clearing and grubbing, temporary sediment and erosion control works, and establishment of environmental and traffic controls.

Earthworks would involve remediation of any existing contamination and removal of any hazardous material, as appropriate, protecting and relocating services, widening of existing rock cuttings (approximately 750 metres of existing cut along the Project would be widened by approximately 20 metres), new cuttings (approximately 1,300 metres of new rock cut would be required to a width of approximately five metres along the Project), and bulk earthworks and haulage. Some of the cutting locations would require retaining walls. Refer to Figure 5.8 for the location of proposed retaining walls in the Project and Figure 5.9 for a representative retaining wall cross section.

Civil and structure works would involve construction of infrastructure, including intersection upgrades, walking and cycling paths, retaining walls, drainage works and pavement works.

Reinstatement would involve implementing traffic management systems and landscaping in accordance with the Landscape Strategy (Technical Report G) for the Project.

5.4.2 Construction laydown areas

To minimise disruption at and around the Project site, one or more separate site compounds (or 'laydown areas') would be established for site offices, storage of materials and plant, amenities for workers, secure container storage, short-term storage for waste and potentially workforce parking. The laydown area(s) would be required to be in use for the full duration of Project construction.

Construction laydown areas have not yet been identified for the Project, other than those included in the project area. Following the engagement of a contractor, they would identify one or more sites that are suitable for this purpose on the basis of minimal environmental impact. Depending on the site(s) selected, a separate planning approval process may be required which would need to be informed by site investigation and consultation.

The project area has allowed for a site on the western side of Yan Yean Road in close proximity to the Yarrambat Horse and Pony Club, which is currently being used as laydown area by Yarra Valley Water. The Project may also utilise the existing Department of Transport owned land at 423-437 Yan Yean Road Yarrambat at the southern end of the project area. Vegetation removal would avoid the no-go zones identified in Attachment VI *Map Book*.

The laydown area(s) would be reinstated following works to their pre-Project condition, or as agreed with the landholder. The nature of reinstatement and any improvement works would be agreed with the landowner and any other relevant stakeholders, potentially Council and the Department of Transport.

5.4.3 Construction method

The construction methods adopted would seek to develop the Project in discrete stages to the extent practicable. This would assist with localising construction impacts for each stage of works. Maintaining traffic flow throughout the Project would be a key component of the construction methodology. Constructing new lanes 'offline' would be integral to maintaining traffic flow, including diverting traffic into new lanes as staged sections were completed. As traffic is diverted into newly constructed lanes, old lanes would be upgraded to assist in maintaining traffic flow.

Temporary road closures and diversions would be required for the construction of intersections. Road closures and diversions would be managed through community consultation and detailed traffic management plans.

Spoil is defined as waste soil or rock resulting from excavation activities. Spoil generated by construction activities would be managed in accordance with EPA requirements applicable at the time of construction.

The final spoil disposal strategy would be developed in accordance with EPA Victoria requirements, particularly in regard to managing any contamination entrained within the soil, and whether spoil would be stockpiled or taken immediately to landfill. Haulage routes would be constrained to arterial roads, including Yan Yean Road. Where roads other than Yan Yean Road or designated arterials are required to be used, this would be done in consultation with the Department of Transport and the relevant local authority, with appropriate notice given to any affected residents.

5.4.4 Working hours

Construction work for the Project would be undertaken in accordance with EPA requirements applicable at the time of construction. Standard construction work hours are:

- Monday to Friday, 7am to 6pm
- Saturday, 7am to 1pm.

Construction outside standard hours might occur at discrete stages to enable particular tasks to be undertaken more safely than could otherwise be achieved. Night works would also be required to minimise impacts on traffic or nearby stakeholders. Works proposed for outside standard hours would need to be approved in advance by MRPV, following consultation with all relevant stakeholders.

5.5 Project operation and maintenance

When complete, Yan Yean Road would be owned by the Department of Transport and operated in accordance with its environmental management approach. Ongoing monitoring and associated management and mitigation measures set out in the EPRs would be implemented during operation of the Project by the relevant organisation.

Maintenance of the infrastructure would be undertaken by Department of Transport, or local Councils for pathways and service roads, in accordance with the *Road Management Act 2004 – Code of Practice*.

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